



City of Bellevue

Development Services Department

Land Use Division Staff Report

Proposal Name: Bellevue DOL Townhomes

Proposal Address: 13133 NE Bellevue-Redmond Road

Proposal Description: Demolition of existing office building and construction of 31 single-family attached (townhome) units.

File Number: 18-108498-LD

Applicant: Justin Goroch/BCRA

Decisions Included: Design Review, and SEPA Threshold Determination (Process II)

Planner: Faheem Darab

State Environmental Policy Act Threshold Determination: Determination of Non-Significance

Carol V. Helland, Environmental Coordinator
Development Services Department

Director's Recommendation:

Approval with Conditions
Michael A. Brennan, Director
Development Services Department

By:

Elizabeth Stead, Land Use Director

Notice of Application: April 26, 2018

Notice of Decision: January 17, 2019

Appeal Deadline for Design Review & SEPA: January 31, 2019

Expiration Date: (if no complete Building Permit application is filed prior to this date - Refer to LUC 20.40.500)

For information on how to appeal a proposal, visit the Development Services Center at City Hall or call (425) 452-6800. Comments on State Environmental Policy Act (SEPA) Determinations can be made with or without appealing the proposal within the noted comment period for a SEPA Determination. Appeal of the Decision must be received in the City Clerk's Office by 5 PM on the date noted for appeal of the decision.

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ATTACHED:

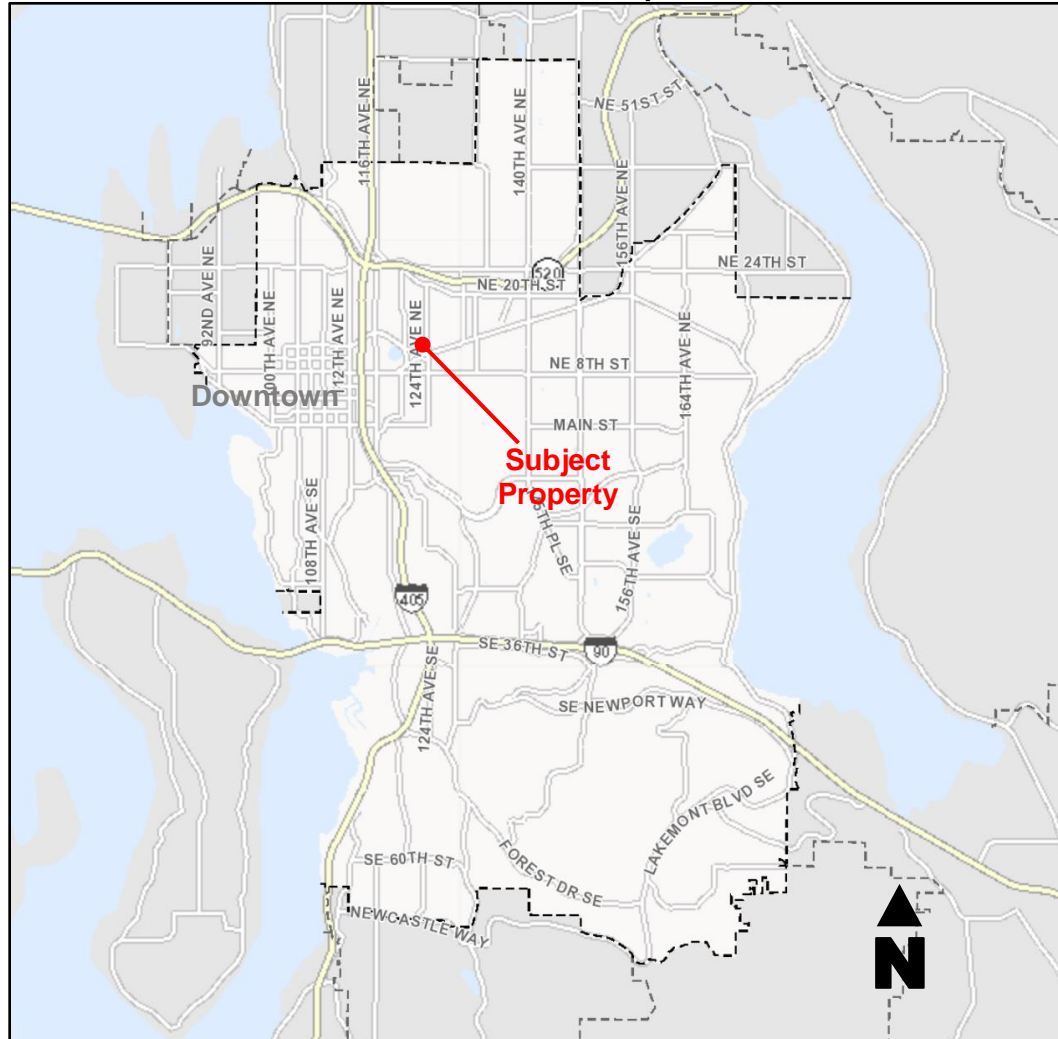
- A. Project Drawings
- B. Determination of Non-Significance
- C. SEPA Checklist

I. REQUEST AND REVIEW PROCESS

A. Request

The applicant requests Design Review approval and a threshold determination under the State Environmental Policy Act (SEPA) to construct 31 single-family attached residential units in 8 buildings on an approximately 1.64-acre (71,509 SF) site in the BR-ORT (BelRed Office Residential Transition) land use district.

Locational Map



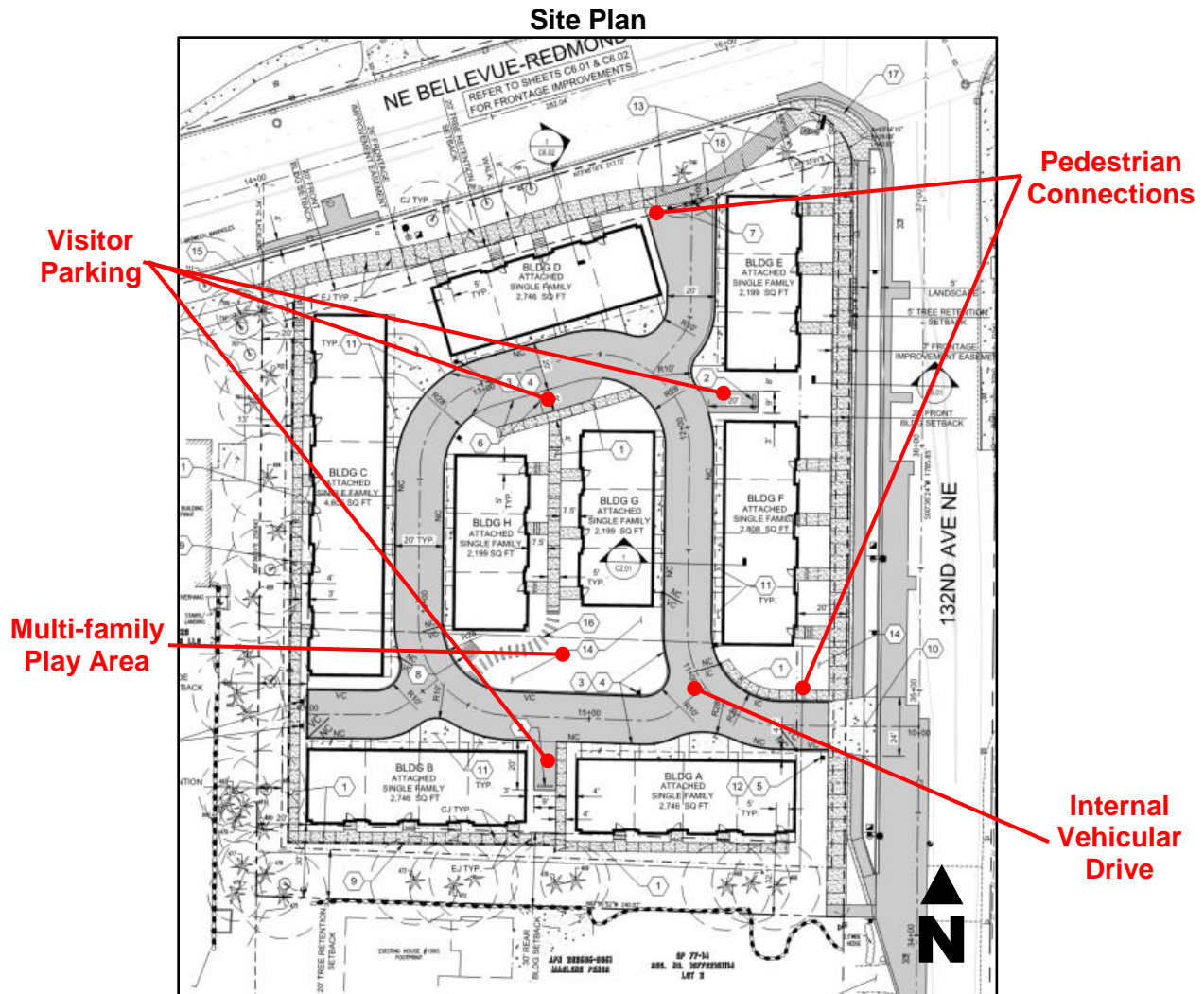
The proposal is to demolish an existing 12,050 square foot office building and associated surface parking currently serving as the Bellevue Department of Licensing office. Each of the proposed 3-story units includes a 2-car garage on the ground floor and average approximately 1,580 square feet (excluding garage space). Other site improvements include a multi-family play area, site landscaping, utilities, street frontage improvements and internal roadways.

B. Review Process

Design Review (LD Permit) is governed by Land Use Code (LUC) 20.30F. The LD and SEPA Determination are all Process II decisions. Process II is an administrative process. The Environmental Coordinator issues the SEPA Threshold Determination and the Director of the Development Services Department issues the LD decision. An appeal of any Process II decision is heard and decided upon by the City of Bellevue Hearing Examiner. **Refer to Condition of Approval regarding modifications to Design Review plans in section XI of this report.**

C. Site Design

The subject property is a single parcel in the southwest corner of the intersection of NE Bellevue-Redmond Road and 132nd Avenue NE. The site address is 13133 NE Bellevue-Redmond Road. The parcel topography is slightly elevated above the surrounding land and generally flat. The parcel is trapezoidal in shape, measuring approximately 240 feet in width and approximately 260 feet in depth on the western property line and approximately 330 feet in depth along the eastern property line.



The existing vehicular access from NE Bellevue-Redmond Road will be closed off

and the only access will be off 132nd Avenue NE. The existing vehicular access from 132nd Avenue NE will be shifted further south. The internal vehicular circulation will be provided through a two-way, circular drive in the center of the site. Individual garages on the ground floor of each unit will front the internal roadway. Four guest parking stalls will be provided.

Pedestrian connections to the site are provided at the northeast from a sidewalk along NE Bellevue-Redmond Road and on the east from a sidewalk along 132nd Avenue NE. Each unit is accessed by sidewalks along the front of each residential unit.

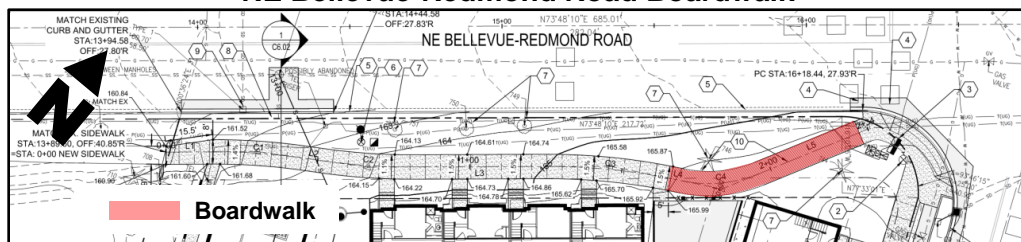
Trash and recycling storage will occur in each unit's garage. **Refer to Condition of Approval regarding solid waste/recycling collection facilities in section XI of this report.**

Streetscape:

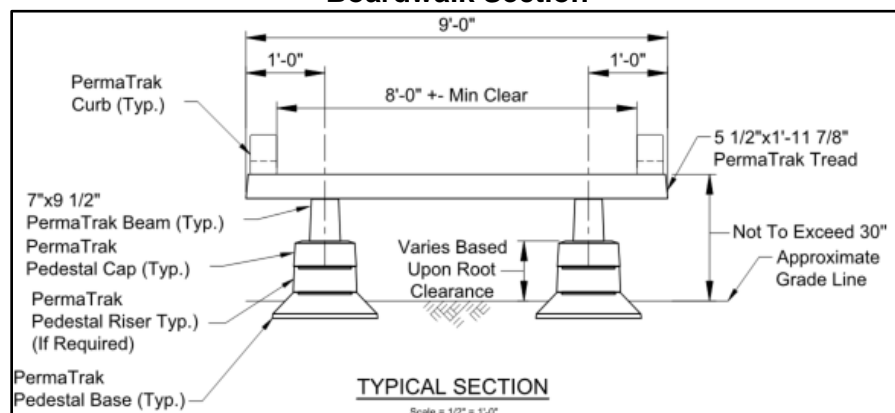
NE Bellevue-Redmond Road:

The streetscape along NE Bellevue-Redmond Road will include a 28-foot wide landscaped area that incorporates a meandering 8-foot wide sidewalk. The meandering provides for a landscape buffer between pedestrians and the high-volume traffic along NE Bellevue-Redmond Road. The meandering also aids in the retention of most of the significant trees. The landscaping includes retained significant trees as well as several newly planted street trees. A boardwalk will span a short distance along the road in order to retain two significant trees. See further discussion of this below in section III.C.

NE Bellevue-Redmond Road Boardwalk



Boardwalk Section

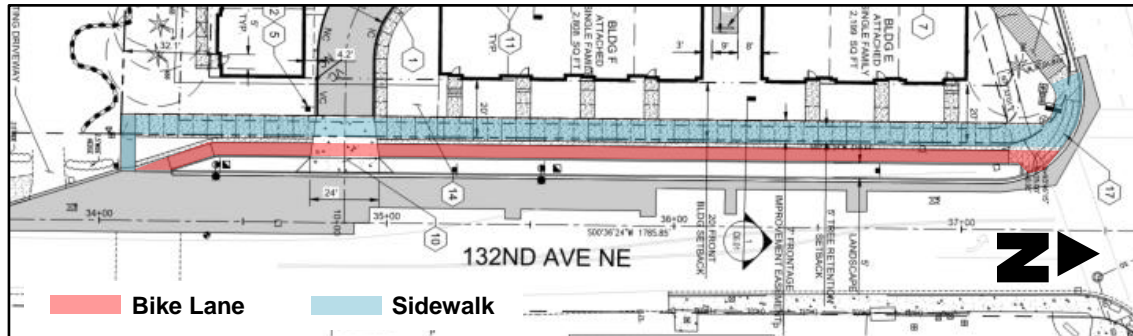


132nd Avenue NE:

The streetscape along the street's west side will include a 5-foot wide landscape strip planted with street trees. Next to this there will be a 5-foot wide concrete bike lane, a

1.5-foot wide concrete wedge providing 6 inches of vertical separation, and an 8-foot wide concrete sidewalk. The existing significant trees along this street frontage must be removed to provide for this public infrastructure.

132nd Avenue NE Buffered Bike Lane and Sidewalk



Landscape and Tree Retention:

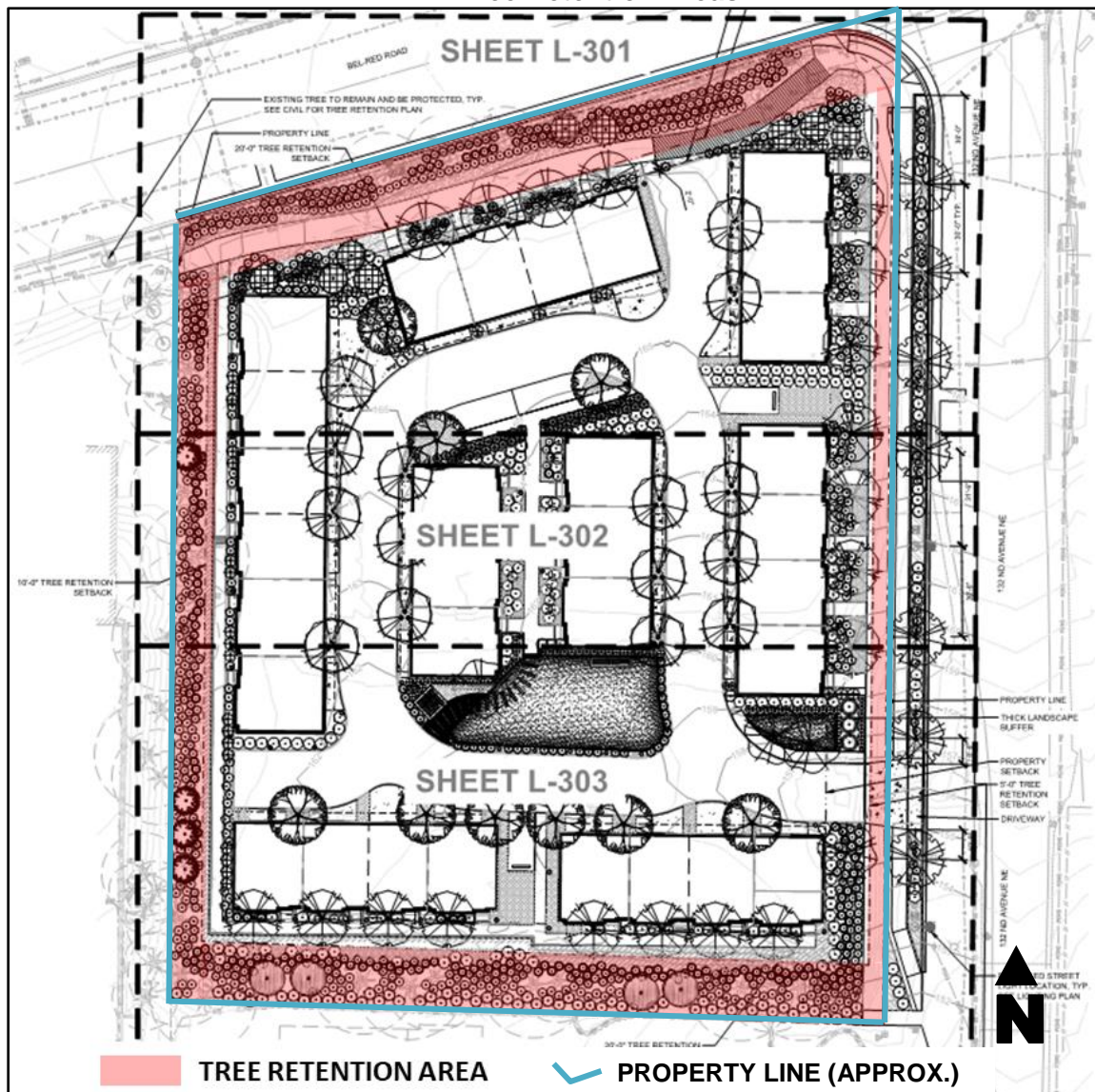
NE Bellevue-Redmond Road requires retention of significant trees within the 20' landscape perimeter according to LUC 20.25D.110.B.3.c and a minimum of five trees per 1,000 square feet of planting area with no more than 40 percent of trees being deciduous. Evergreen shrubs and living ground cover are required as well. Overall, at least 50 percent of plantings must be native species. Most significant trees are being retained, with several being removed due to required public sidewalks. Nine evergreen trees (Douglas Fir) are being planted to meet the requirements.

132nd Avenue NE requires retention of significant trees within the 5' perimeter according to LUC 20.25D.110.B.3.e but due to required public infrastructure, all existing significant trees are being removed in accordance with LUC 20.20.900.D.1. Forest Green Oaks are being planted for street trees along the landscape strip abutting the street, along with Vine Maple trees in each unit's front yard. Oakleaf Hydrangea and Western Sword Ferns are being planted in the landscape strip along with Beach Strawberry groundcover.

The interior property line along the west is required to retain significant trees within the 10' perimeter. Most existing significant trees are being retained, with several being removed due to proposed grading and sidewalks to access unit front doors. As an Alternative Tree Replacement Option, they are being replaced by four evergreen trees (Incense Cedars) to meet the requirements of LUC 20.20.900.D and G.

The southern property line is considered “perimeter” due to the site being BelRed ORT (office-residential transition) abutting a single family residential district to the south. This 20-foot perimeter is required to retain significant trees. All significant trees are being retained. Four evergreen trees (Western Red Cedars) are being planted to meet the landscaping requirements of LUC 20.25D.110.C.3.

Tree Retention Areas



D. Building Design

The architectural form and materials are contemporary and provide a simple, clean look with a few but highly contrasting colors and materials. The form and materials include simple shapes and colors that pull the eye both vertically and horizontally. The materials and colors provide a clean, modern look that is softened by natural materials and earth tones.

Form:

Flat rooflines and eaves, slightly offset to account for slight changes in topography, provide horizontal direction. Projections of portions of the first and second floors provide vertical direction which when combined with the dark gray non-projecting portions of the building further accentuate the vertical. The form's mass is broken down by the connection of 6 residential units or less. In most cases, there are only 3 or 4 units connected.

Materials/Colors:

Dark walnut colored aluminum siding, dark gray aluminum balcony railings and cementitious lap siding, and decorative cedar wood slats all provide horizontal direction to the design. The garage door's dark gray muntin framing contrasts strongly with its white frosted glass to further accentuate the overall horizontal/vertical design aesthetic.

The fiber cement board and cementitious lap siding are contemporary materials and their shades of gray highlight the modern design aesthetic. But this is balanced with natural materials and earth tones including cedar wood material, dark walnut colored aluminum siding and reddish brown painted wooden front doors. The natural materials will help to blend the design in with the extra landscaping required by the BelRed ORT land use district as well as the lower density single family detached neighborhood to the south.

Typical Front Elevation



Typical Rear Elevation

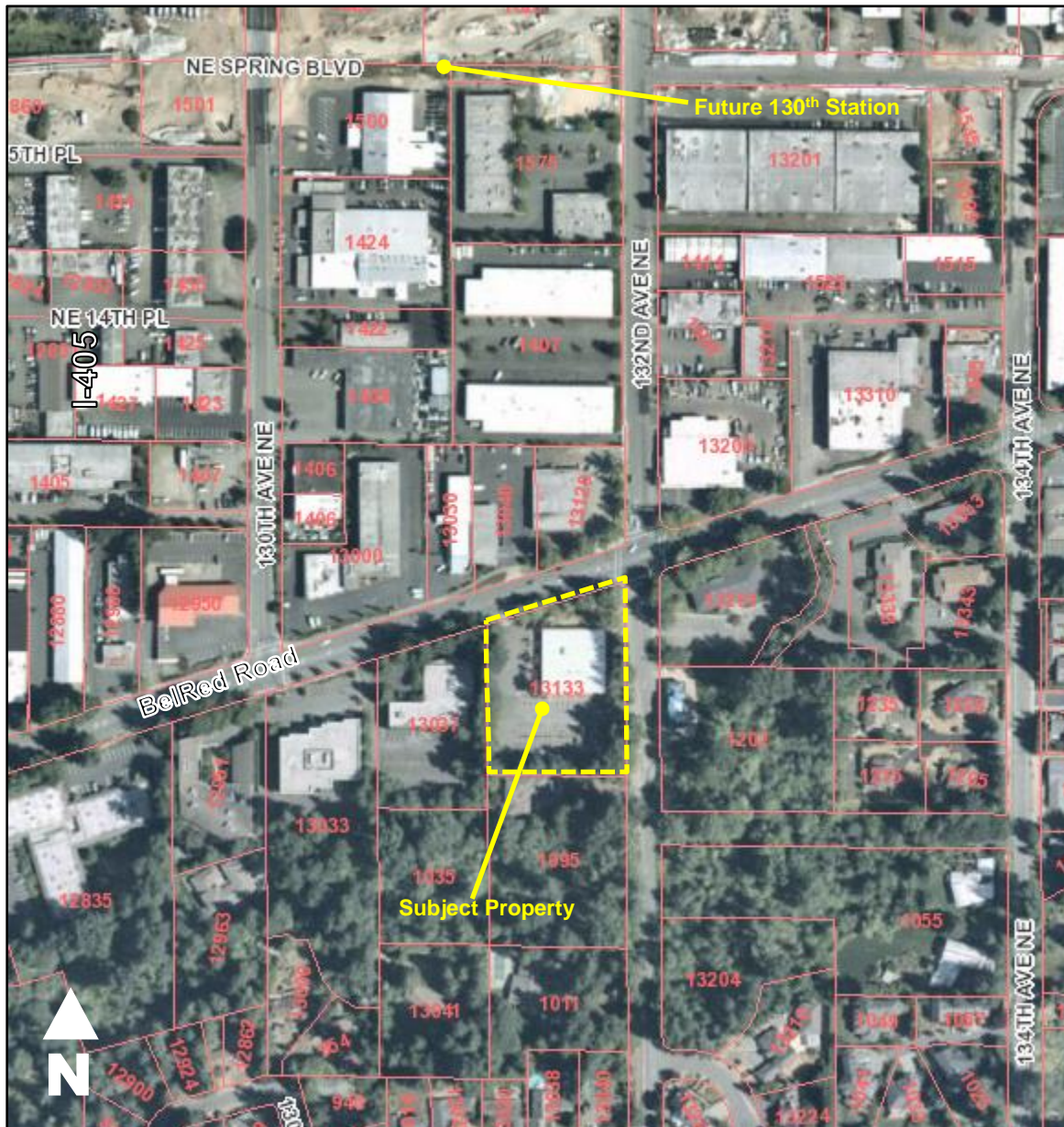


II. SITE DESCRIPTION, ZONING, AND LAND USE CONTEXT

A. Site Description

The site is at the southern edge of the BelRed Subarea roughly halfway between Downtown/I-405 and Redmond City limits or about a mile from each. The site is less than a quarter mile south of the future 130th Station of the East Link light rail extension. The subject property is a single parcel of approximately 1.64 acres in the southwest corner of the intersection of NE Bellevue-Redmond Road and 132nd Avenue NE. The site address is 13133 NE Bellevue-Redmond Road.

Context Aerial

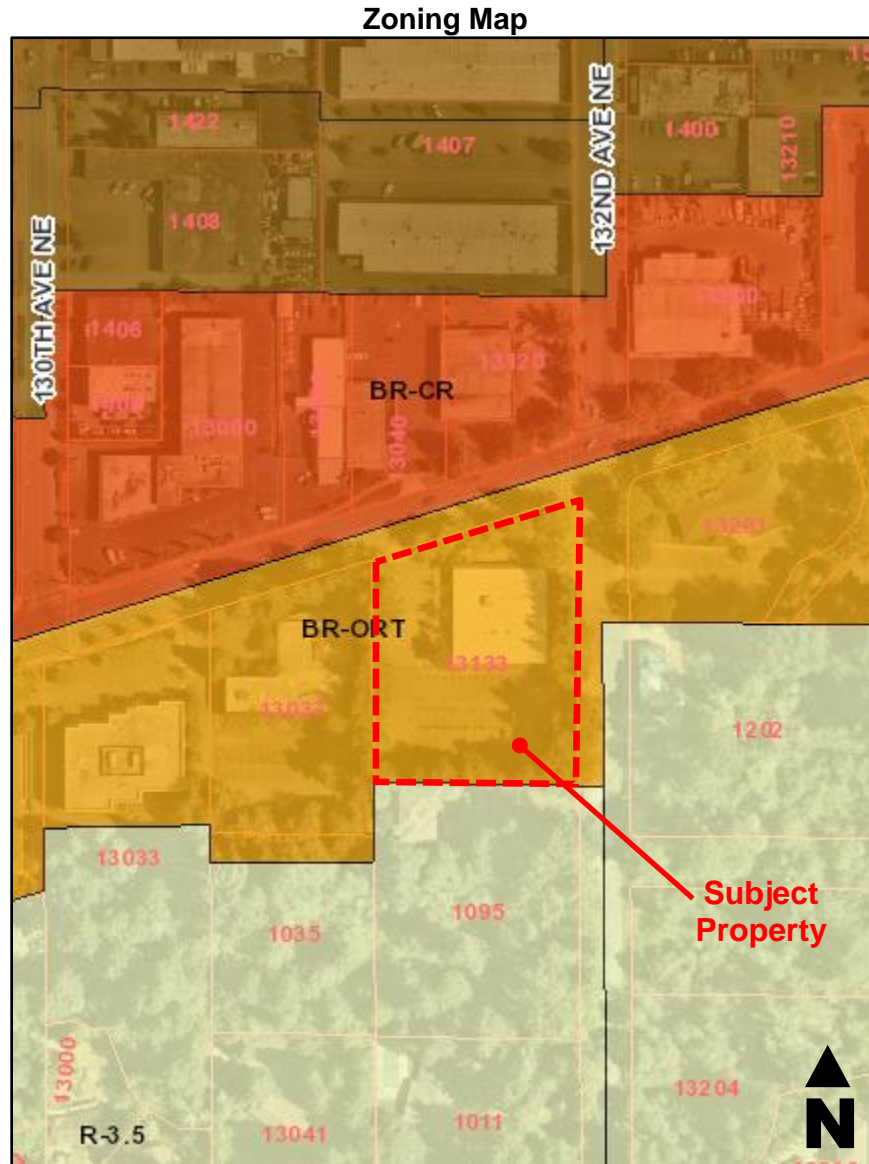


B. Zoning and Context

The site is zoned BelRed-Office/Residential Transition (BR-ORT). The purpose of the BR-ORT land use district is to provide an area for low-intensity offices and uses and low density multifamily residential dwellings that are intended to be developed in such a manner as to provide a buffer between residential neighborhoods to the south and more intensively developed properties within BelRed.

Surrounding land use districts (zoning) and existing land uses are:

- North: A mix of retail, light industrial and service uses including paint supply, equipment rental, and auto repair shops as well as a dog day care (BelRed-Commercial/Residential).
- South: Single family detached residential (R-3.5 and 2.5).
- West: 2-story office use (BR-ORT).
- East: 2-story office use (BR-ORT).



III. CONSISTENCY WITH LAND USE CODE/ ZONING REQUIREMENTS

A. General Provisions of the Land Use Code

1. Use

Uses are regulated by LUC 20.25D.070 (Use Charts). The proposed residential use is permitted in the BR-ORT land use district.

2. Dimensional Requirements

All applicable dimensional requirements of the Land Use Code will be met. Refer to the following chart for specific information, as well as Conditions of Approval and the attached Project Plans.

Category	Required	Proposed / Comments
Land Use Designation	BelRed ORT	BelRed ORT
Project Limit	N/A	71,509 square feet
Building Height	45 feet permitted, measured from average existing grade LUC 20.25D.080.A	39.92 feet for tallest building Meets LUC requirement
Maximum Impervious Surface	75% or 53,632 square feet LUC 20.25D.080.A	60% or 43,042 square feet Meets LUC requirement
Floor Area Ratio (FAR)	0.75 FAR base 0.75 FAR maximum LUC 20.25D.080.A	0.69 FAR or 49,034 square feet Meets LUC requirement
Structure Setbacks	<u>Front</u> : 20 feet from both NE Bellevue-Redmond Road and 132 nd Avenue NE <u>Rear</u> : 30 feet from southern property line <u>Side</u> : 20 feet from western property line LUC 20.25D.080.A	Meets LUC requirement
Parking	Vehicular Parking: Minimum: 1.0 per unit Maximum: 2.0 per unit Visitor Spaces: Not required Bike Parking: 1 space per 10 dwelling units (31 units requires 3.1 spaces) Minimum 50% of bike spaces should be covered. LUC 20.25D.120	Vehicular Parking: 2.0 spaces per unit provided within each unit's ground floor for a total of 62 spaces. Visitor spaces: Four spaces provided Bike Parking: 4 spaces, 100% covered <u>Refer to Conditions of Approval regarding bicycle parking in Section XI of this report.</u>

	Permitted/Required	Proposed/Comments
Tree Retention	<p><u>Site Perimeter:</u> 100% of trees 8-inch diameter or greater must be retained within the required landscape areas. Areas devoted to access and site areas, sidewalks, trails, etc. are exempt from this requirement.</p> <ul style="list-style-type: none"> • NE Bellevue-Redmond Road landscape area is 20 feet • 132nd Avenue is 5 feet • Interior (along west) is 10 feet • Perimeter (along south) is 20 feet <p>There are 38 significant trees within site perimeter.</p> <p><u>Site Interior:</u> 15% of 474 interior diameter inches retained = 71 inches</p> <p>LUC 20.25D.110.F and 20.20.900.D</p>	<p><u>Site Perimeter:</u> 63% of the perimeter's significant trees will be retained (24 trees of 38). A total of fourteen (14) significant trees are proposed to be removed. Two (2) due to their poor condition according to the arborist report and twelve (12) due to City required frontage improvements such as sidewalks, bicycle facilities, and traffic signals.</p> <p>Tree mitigation planting was required to meet landscaping requirements:</p> <ul style="list-style-type: none"> • 4 Incense Cedars along the west • 4 Western Red Cedars along the south • 9 Douglas Fir along the north <p><u>Site Interior:</u> 22% (103 diameter inches) retained.</p> <p><u>Refer to discussion below in Section III.C regarding the Tree Retention/Boardwalk Solution and Conditions of Approval regarding the final landscape plan, tree protection, and tree mitigation planting in Section XI of this report.</u></p> <p>As conditioned, meets LUC requirement.</p>
Landscape	<p>Streetscape planting strips:</p> <p>NE Bellevue-Redmond Road: 20 feet width minimum</p> <p>132nd Avenue NE: 5 feet width minimum</p> <p>Perimeter property lines: 20 feet width minimum along the southern property line</p> <p>Interior property lines: 10 feet width minimum along the western property line</p> <p>LUC 20.25D.110</p>	<p>Meets LUC requirements</p> <p>NE Bellevue-Redmond Road:</p> <ul style="list-style-type: none"> • 20 feet • Douglas Fir <p>132nd Avenue NE:</p> <ul style="list-style-type: none"> • 5 feet • Forest Green Oak <p>Perimeter property line (to the south):</p> <ul style="list-style-type: none"> • 20 feet • Western Red Cedar <p>Interior property line (to the west):</p> <ul style="list-style-type: none"> • 10 feet • Incense Cedar <p><u>Refer to Conditions of Approval regarding street trees and right-of-way landscaping, landscape installation assurance device, and landscape maintenance assurance device in Section XI of this report.</u></p>

Category	Permitted/Required	Proposed/Comments
Mechanical Equipment	Located on the roof & visually screened, within the building or at grade and visually screened. LUC 20.20.525	None proposed with this application. <u>Refer to Condition of Approval regarding addition of air conditioning units in Section XI of this report.</u>
Lighting	All exterior lighting will require cut-off shields to protect adjacent neighborhoods. LUC 20.20.522	<u>Refer to Condition of Approval regarding lighting in Section XI of this report.</u> As conditioned, meets LUC requirements.

B. Transition

The BelRed ORT land use district provides further transition to adjacent single-family residential land use districts to the south.

Structure Setbacks: In contrast to other land use districts in the BelRed, ORT requires buildings setback at least 20' from the right of way along NE Bellevue-Redmond Road and 132nd Avenue NE. Buildings along the southern perimeter are set back 30-feet from the property line and those along the west on 132nd Avenue NE are set back 20 feet. All of this provides for space and buffering between the site and adjacent uses. See LUC 20.25D.080.A.

Development Standards: The development standards for this land use district are described in LUC 20.25D.130.D and have been met with this proposal. The building design provides for facade stepbacks, roof overhangs and recesses for each unit. The building materials include natural materials like cedar and neutral colors. In addition, buildings are spaced a minimum of 20' apart.

Landscape: A 20-foot wide landscape buffer is provided along the southern perimeter to buffer the lower density residential uses to the south from the more dense uses on site. A 10-foot wide landscape buffer is provided on the western perimeter to buffer the different properties from each other. See LUC 20.25D.110.C and D.

C. Tree Retention / Boardwalk

Two significant trees along the NE Bellevue-Redmond Road frontage are proposed to be retained through the innovative provision of a boardwalk structure in lieu of the typical concrete sidewalk that would result in the killing and removal of these trees. The boardwalk's construction method is intended to minimize the disruption to the root system of the trees in order to ensure their survival. The proposed boardwalk drives pins into the soil to support the structure. This method minimizes disruption to the root systems. The boardwalk has been carefully designed during this Design Review to ensure that it is buildable and will maintain ADA accessibility and an 8' wide clear space. Refer to Conditions of Approval regarding the boardwalk in Section XI of this report.

IV. DESIGN GUIDELINES AND DESIGN STANDARDS

Each development within a BelRed land use district must comply with the provisions of the BelRed Subarea Design Guidelines LUC 20.25D.150.

A. Character and Site

1. Integrate the Natural Environment

Finding: A high-quality boardwalk is being installed along a portion of NE Bellevue-Redmond Road near its intersection with 132nd Avenue NE. The boardwalk design will serve to protect two significant trees, Douglas Firs, that otherwise would need to be removed. The materials will be high-quality composite decking and concrete beams. In addition, PNW adapted plantings will be utilized to integrate with the surrounding trees and shrubs.

2. Promote Architectural Compatibility

Finding: The building design is similar to several recent townhouse projects built along NE Bellevue-Redmond Road. The modern design combines a clean, minimalist look with natural materials and colors to fit into the surrounding architectural context along NE Bellevue-Redmond Road and with the neighborhood to the south.

B. Pedestrian Emphasis

3. Enhance the Pedestrian System

Finding: The project design prioritizes pedestrians in several ways. Along NE Bellevue-Redmond Road, a landscaped buffer of between 10-20 feet protects pedestrians from the roadway's traffic. Pedestrian connections from the site onto NE Bellevue-Redmond Road and 132nd Avenue NE conveniently connect pedestrians to the surrounding network and the future light rail to the north. Along 132nd Avenue NE pedestrians and cyclists are separated from each other to enhance each users' safety. A signal cabinet and signal pole at the intersection of NE Bellevue-Redmond Road and 132nd Avenue NE are being relocated to improve pedestrian flow and safety. To improve the aesthetics of the development for pedestrians and others, extra landscaping and fencing will shield pedestrian views along the two roads from the alleyways within the project site. Most existing significant trees along NE Bellevue-Redmond Road will be retained which will provide pedestrians with comfort and shade.

4. Create a Variety of Successful Outdoor Spaces

Finding: An outdoor multi-family play area is proposed central to the project that will provide a green space for play, along with seating, lighting and landscape buffers along the edges to protect children from vehicles in the adjacent drive aisles.

C. Architectural

5. Encourage High Quality Materials

Finding: The architectural form and materials are contemporary and provide a simple, clean look with few but highly contrasting colors and materials. The form and materials include simple shapes and colors that pull the eye both vertically and horizontally. The materials and colors provide a clean, modern look that is softened by natural materials and earth tones. Some materials include decorative cedar wood slats, aluminum railings, and frosted glass.

6. Create Attractive Building Silhouettes and Rooflines

Finding: Flat rooflines and eaves, slightly offset to account for slight changes in topography, provide horizontal direction. These offsets also provide for varied rooflines. Projections of portions of the first and second floors provide vertical direction which when combined with the dark gray non-projecting portions of the building further accentuate the vertical.

D. Lighting

7. Orient Lighting Towards Sidewalks and Public Spaces

Finding: Pedestrian-scaled lighting along the projects pathways, intersections with public sidewalks and along the internal play area provide for pedestrian safety and comfort.

V. PUBLIC NOTICE AND COMMENT

Design Review / SEPA

Application Date: March 19, 2018

Application Completeness Date: April 09, 2018

Notice of Application published: April 26, 2018

Public Notice Sign installed: April 26, 2018

Minimum Comment Period ended: May 10, 2018

A public meeting for this Design Review/SEPA Determination was held on May 15, 2018 at City Hall and there were no attendees.

Although the minimum required public comment period ended on May 10, 2018 for the Design Review and SEPA Threshold Determination, comments were accepted up to the date of this decision. No written comments were received for the Design Review application nor the SEPA Threshold Determination.

VI. TECHNICAL REVIEW

A. Clearing & Grading

The clearing and grading reviewer has reviewed the plans and materials submitted for this project and has approved the clearing and grading portion of the design review application. Approval of this design review does not constitute an approval of any construction permit. An application for a clearing and grading permit must be submitted and approved before construction can begin. Plans submitted as part of any permit application for this project shall be consistent with the activity permitted under this approval and must comply with the City of Bellevue Clearing and Grading Code (BCC 23.76).

B. Utilities

Water

The water supply for this site will be provided from the existing 8" water main located in 132nd Ave NE off the 400 water pressure zone. Flow modeling continues and the fire flow to this area is 1,500 gpm.

Sewer

The City of Bellevue Utilities has adequate capacity for providing sanitary sewer for this proposal. The site will connect to the existing 8" sewer main located in NE

Bellevue-Redmond Rd.

Storm Drainage

The proposal will not increase the rate of runoff of storm water leaving the site. The City of Bellevue Surface Water Engineering Standards provide adequate requirements for this proposal.

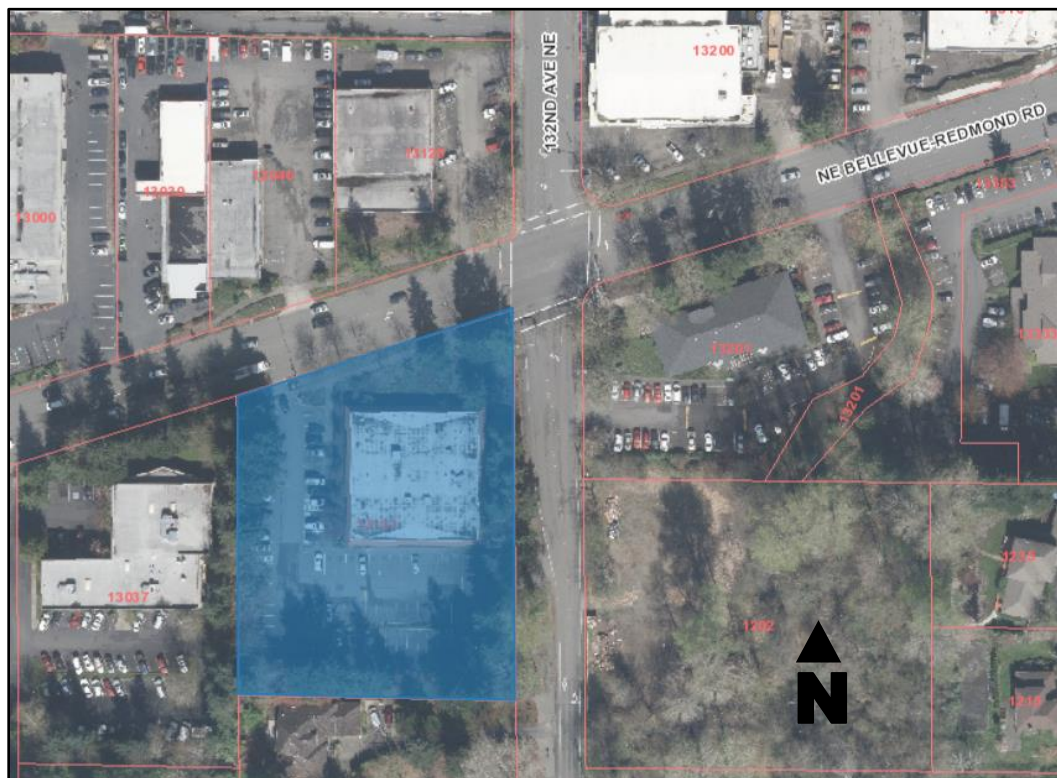
The storm drainage system proposed provides adequate mitigation for storm water and meets the requirements outlined in the Bellevue Code and the Storm & Surface Water Engineering Standards.

Refer to Conditions of Approval regarding utilities in Section XI of this report.

C. Transportation

Project Summary

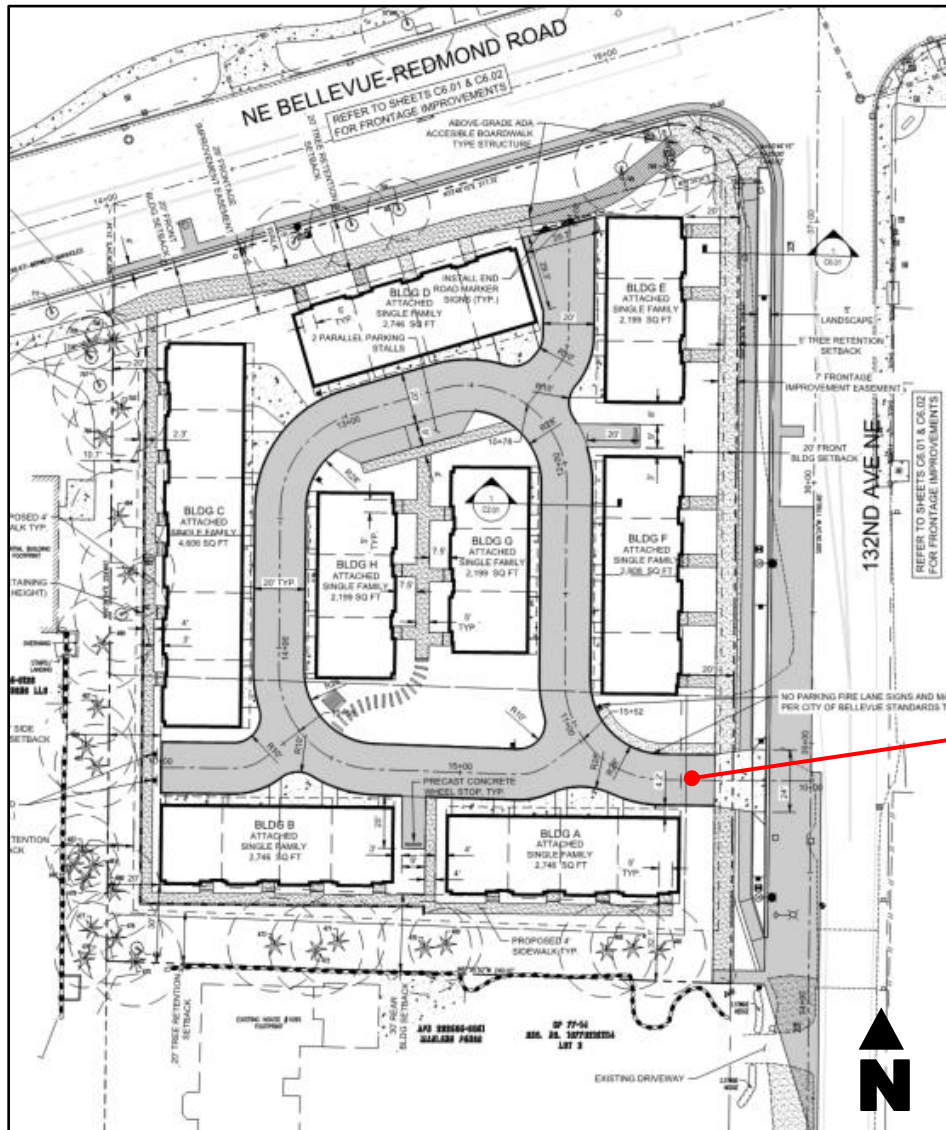
The proposed project is located at the intersection of 132nd Avenue NE and NE Bellevue-Redmond Road in the SW corner of the intersection, shaded in blue below. The project is located approximately 850 feet south of a BelRed light rail station that will be located between 130th and 132nd Avenues NE. 132nd Avenue NE is a priority bike corridor where development will construct one way protected bike lane facilities along that frontage.



The existing site is a Washington State Department of Licensing office. The office that is to be demolished is 12,050 square feet, and it estimated to have generated approximately 206 peak hour trips between 4pm and 6pm. The new project proposes to construct 8 buildings and a total of 31 multi-family units that are estimated to generate 17 peak hour trips between 4pm and 6pm for a net reduction of 189 peak hour trips between 4pm and 6pm.

Site Access and Loading

The site has two existing access locations, one onto NE Bellevue-Redmond Road with no turn restrictions and a second onto 132nd Avenue NE with no turn restrictions. The proposed project will remove the existing access onto NE Bellevue-Redmond Road and reconstruct a new access onto 132nd Avenue NE as shown below near the SE corner of the site.



The project proposes a minimum 20-foot wide private internal loop road that allows for deliveries, residential move-in/move-out, and garbage to be accommodated on-site without backing onto the public road. On-street loading will not be allowed or permitted. The loop road will be marked as a fire lane.

Refer to Conditions of Approval regarding provisions for loading in Section XI of this report.

Impact Mitigation and Frontage Improvements

In order to provide safe pedestrian and vehicular access in the vicinity of the site, and to provide infrastructure improvements with a consistent and attractive appearance, the construction of street frontage improvements is required as a condition of development approval. The design of the improvements must conform to the requirements of the Americans with Disabilities Act, the Transportation Development Code (BCC 14.60), and the provisions of the Transportation Department Design Manual.

Engineering and construction details must be shown on the civil engineering plans submitted to the clearing and grading permit. The engineering plans shall be the controlling document on the design of these features; architectural and landscape plans must conform to the engineering plans. During construction, city inspectors may require additional survey work at any time to confirm proper elevations. The building grade and elevations shall be consistent with the curb and sidewalk grade shown in the approved civil engineering plans.

There is an existing 6-foot wide sidewalk along Bellevue-Redmond Road and no sidewalk on 132nd Avenue NE. There is a non-compliant ADA ramp to cross 132nd Avenue NE and no existing ADA ramp to cross Bellevue-Redmond Road. Pedestrian push buttons are also non-compliant in their existing condition. The frontage improvements will require sidewalks on both that meet the arterial road classification and a protected bike facility on 132nd Avenue NE. 132nd Avenue NE is a bike corridor that runs north and south from the light rail station and is listed in the 2009 pedestrian bike plan. The corner will be reconstructed to be ADA compliant by the developers with the City providing specific materials listed in the section below.

1. NE Bellevue-Redmond Road

- a. A minimum 8-foot wide concrete sidewalk
(The Transportation Department will allow the construction of a concrete board walk along a portion of the frontage to save a specific tree, if it can be constructed to be ADA compliant. If this boardwalk cannot be constructed to be ADA compliant a concrete sidewalk will be required in its place)
- b. A minimum 20-foot wide landscape planter, the sidewalk may meander through the planter but is not counted towards the planter width requirement.
 - The planter strip shall have street trees 20-ft on-center, shrubs, ground cover, spray irrigation, root barrier, and soil preparation for the street trees.
- c. Street lighting that meets Bellevue Standards.
 - Black poles, arms and fixtures
- d. New concrete curb and gutter shall be installed only at the location of the existing driveway that is to be removed, as required for the reconstructed corner, and at the location of any remaining curb and gutter that is damaged during construction.

2. 132nd Avenue NE

- a. A minimum 8-foot wide concrete sidewalk
- b. A minimum 18-inch concrete wedge or equivalent for grade separation between the concrete sidewalk and the Hot Mix Asphalt (HMA) southbound bike lane.
- c. A minimum 5-foot wide HMA bike lane
- d. A flush vertical curb between the bike facility and the planter, this may be

included in the required 5-foot bike lane width.

- e. A minimum 5-foot wide planter
 - The planter strip shall have street trees 20-ft on-center, shrubs, ground cover, spray irrigation, root barrier, and soil preparation for the street trees.
- f. Street lighting that meets Bellevue Standards
 - Black poles, arms and fixtures
- g. A minimum of 17-feet of HMA pavement from the face of curb to the existing centerline of ROW. The use of existing pavement may be used towards meeting this requirement.
- h. Transitions for the sidewalk and protected bike lane back into the existing street section to the south
- i. Channelization and signage within the road and behind the curb, within the protected southbound bike lane and sidewalk.
- j. New concrete curb and gutter

3. Intersection of 132nd Avenue NE and NE Bellevue-Redmond Road (The SW Corner)

- a. Reconstruct the corner with new signal equipment and an ADA compliant ramp that accommodates the N/S protected bike lane. The preliminary ramp design will require radial cast iron domes with a thickened concrete section where vehicles may track onto the truncated domes.
 - The City shall provide the developer the following signal components: Terminal Cans, Type 1 Poles, Type PS poles, Type III signal Pole with Mast Arm with Anchor Bolts, Signal Heads, Pedestrian Clams, D-Mount, the Luminaire, APS Push Buttons, and APS CCU if needed. The labor, signal pole foundation, and any other materials not specifically listed above is the responsibility of the developer. If the City does not provide the specific materials listed above, the requirement to install new signal equipment at the corner will be waived and the current signal equipment will remain as is. This waiver does not apply to ADA or the need to reconstruct the corner to meet ADA to the maximum extent feasible.
- b. Street lighting that meets Bellevue Standards
 - Black poles, arms and fixtures
- c. Install a new signal pole/mast arm with ADA compliant push buttons, pedestrian heads.

4. The Americans with Disabilities Act (ADA) requires that sidewalk cross slopes not exceed two percent. The sidewalk cross slope may be less than two percent only if the sidewalk has a longitudinal slope sufficient to provide adequate drainage. Bellevue's standard for curb height is six inches, except where curb ramps are needed. The engineering plans must comply with these requirements, and must show adequate details, including spot elevations, to confirm compliance. New curb and sidewalk shall be constructed in compliance with these requirements. Building elevations shall be consistent with the required curb and sidewalk elevations. Spot elevations must be included in the building plans in a manner that proves that building elevations are designed to correspond to the sidewalk elevations shown in the engineering plans, especially at entrances and other key points. Curb and sidewalk elevations will not be revised to fit the building, and city inspectors may require spot surveys during construction in order to confirm the required elevations.

ADA also requires provision of a safe travel path for visually handicapped pedestrians. Potential tripping hazards are not allowed in the main pathway. Any planter boxes installed in the sidewalk to improve pedestrian sight distance at driveways must be designed to reduce the tripping potential and must not extend more than two feet into the public sidewalk. Traffic signal controller boxes and streetlight contactor cabinets must be located so as not to interfere with the main pedestrian path. Buildings shall be designed so that doors do not swing out into the pedestrian path. Installation of colored or textured bands to guide pedestrians in the direction of travel is advisable, subject to the requirements for non-standard sidewalk features. ADA-compliant curb ramps shall be installed where needed, consistent with City and WSDOT standard drawings. If such standards cannot be met, then deviation from standards must be justified on a Design Justification Form to be filed with the Transportation Department.

5. To the extent feasible, no new utility vaults that serve only one development will be allowed within a public sidewalk. Vaults serving a broader public purpose may be located within a public sidewalk. To the extent feasible, no utility vaults may be located within the primary walking path in any sidewalk. All existing vault lids shall be replaced to be non-slip.
6. No fixed objects, including fire hydrants, trees, and streetlight poles, are allowed within ten feet of a driveway edge, defined as Point A in standard drawing SW-140-1 or equivalent. Fixed objects are defined as anything with breakaway characteristics greater than a four-inch by four-inch wooden post.
7. No new overhead utility lines will be allowed within or across any right of way or sidewalk easement, and existing overhead lines must be relocated underground.
8. A private road street name sign is required to be installed. The applicant has the option of paying the City to manufacture the street name sign or having one manufactured by a private vendor. The applicant is responsible for procuring the sign post and hardware and is responsible for installation. The specifics of the street name sign will be determined during the clearing and grading permit.
9. The applicant is required to coordinate mailbox location with the Bellevue Postmaster and show the mailbox location on the engineering plans within the site.

Refer to Conditions of Approval regarding frontage improvements and civil engineering plans, and building and site plans in Section XI of this report.

Easements

The applicant shall provide sidewalk and utility easements to the City as needed to encompass the full required width of the required frontage improvements.

The applicant shall provide easements to the City for location of signal and street light facilities consisting of above-grade boxes and/or below-grade vaults between the building and sidewalk within the landscape area on the projects frontages.

Transformers and utility vaults to serve the building shall be placed inside the building or below grade, to the extent feasible.

Refer to Conditions of Approval regarding existing easements, easements for signal control and street light boxes and vaults, and sidewalk/ utility easements in Section XI of this report.

Right of Way Dedication

To incorporate street improvements which are reasonably necessary to mitigate the direct results of the development, and to accommodate the street widening described elsewhere in this document, the developer is required to dedicate property such that street surface to back of curb is accommodated within the public right of way. This is anticipated at the intersection of 132nd Avenue NE and NE Bellevue-Redmond Road. **Refer to Conditions of Approval regarding dedication of right-of-way in Section XI of this report.**

Use of the Right of Way During Construction

Applicants often request use of the right of way and of pedestrian easements for materials storage, construction trailers, hauling routes, fencing, barricades, loading and unloading and other temporary uses as well as for construction of utilities and street improvements. A Right of Way Use Permit for such activities must be acquired prior to issuance of any construction permit including demolition permit. Sidewalks may not be closed except as specifically allowed by a Right of Way Use Permit. **Refer to Conditions of Approval regarding right-of-way use permit in Section XI of this report.**

Pavement Restoration

The City of Bellevue has established the Trench Restoration Program to provide developers with guidance as to the extent of resurfacing required when a street has been damaged by trenching or other activities. Under the Trench Restoration Program, every street in the City of Bellevue has been examined and placed in one of three categories based on the street's condition and the period of time since it has last been resurfaced. These three categories are, "No Street Cuts Permitted," "Overlay Required," and "Standard Trench Restoration." Each category has different trench restoration requirements associated with it. Damage to the street can be mitigated by placing an asphalt overlay well beyond the limits of the trench walls to produce a more durable surface without the unsightly piecemeal look that often comes with small strip patching.

Near this project, NE Bellevue-Redmond Road is classified as Standard Trench Restoration and 132nd Avenue NE is classified as Grind and Overlay Restoration.

Refer to Conditions of Approval regarding pavement restoration in Section XI of this report.

D. Fire

The Bellevue Fire Department, Fire Prevention Division has reviewed the submittal in accordance with the 2015 International Fire Code, 2015 International Building Code, City of Bellevue requirements, and good fire protection practices. This review was based upon and limited to the information presented on drawings and/or materials received in our office. The Fire Department can approve the application, subject to conditions, which includes marking the internal access roads as fire lanes. **Refer to Conditions of Approval regarding Fire Department in Section XI of this report.**

E. Building Division

The plans submitted for Design Review approval dated March 19, 2018, have not been sufficiently developed for a thorough review under the 2015 IRC (International Residential Code), including amendments made by the State of Washington and the City of Bellevue. Complete review will occur under the Building permit application(s) in accordance with the codes in effect at the time of complete application. The following comments are preliminary in nature and are not intended to be all-inclusive or imply approval.

The plans generally conform to the level of detail typical at this stage in the design process. The following items are required to be addressed in the development of the plans for building permit.

The site plan indicates the development of eight buildings on a single lot consisting of a total of 31 three-story residential townhomes. Since each residential unit is not located on a single platted lot, the project is subject to the requirements of RCW 64.55. State law requires, in RCW 64.55.020, that the City not issue a permit until the designer of a multiunit residential building enclosure has submitted plans and details of the building enclosure and a stamped statement that substantially reads: "The undersigned has provided building enclosure documents that in my professional judgment are appropriate to satisfy the requirements of sections 1 through 10 of [RCW 64.55]."

Exterior wall ratings, opening protection, and projections are required to be addressed in accordance with IRC Section 302.1 and 302.2 based on the fire separation distance. Fire separation distance is defined as the distance measured from the building face to one of the following (the distance is required to be measured at right angles from the face of the building wall).

1. Closest interior lot line
2. Centerline of a street, alley or public way
3. An imaginary line between two buildings on the lot

At least one accessible route within a site is required to be provided from public transportation stops, accessible parking, accessible passenger loading zones, and public streets or sidewalks to the accessible building entrances served. The drawings are not currently developed to a sufficient level for us to determine if accessible route requirements are met. Building permit drawings and documents are required to demonstrate that accessible route requirements will be met. IBC 1104.1

Parking within the footprint of the townhouse dwelling unit (garage) is limited to the private parking for that unit only. **Refer to Conditions of Approval regarding accessible routes and private parking (within units) in Section XI of this report.**

F. Addressing

The City of Bellevue will designate a street name to each internal street segment. All north-south road sections will be named independently of east-west trending road sections, even if the roads appear to be continuous.

Addresses for the project will be assigned per Bellevue City Code Chapter 14.02 Names and Numbers. Based on the application materials submitted as part of permit 18 108498LD, addresses for this project (including the building shell address and residential addresses) will be assigned from the internal private road segments.

Each building will require its own address, independent of the parcel address. Address assignments will depend on the final layout and design of the building. Each townhome unit will be with provided its own street address. The townhomes will not use unit numbers as part of their official address. **Refer to Condition of Approval regarding addressing in Section XI of this report.**

VII. STATE ENVIRONMENTAL POLICY ACT (SEPA)

The environmental review indicates no probability of significant adverse environmental impacts occurring as a result of the proposal that are beyond those identified in the BelRed Corridor EIS and the Transportation Facilities Plan EIS. The Environmental Checklist together with information submitted (in the official file) adequately discloses expected environmental impacts associated with the proposed Design Review approval. The City codes and requirements, including SEPA, Land Use Code, Noise Ordinance, Building Code and other construction codes adequately mitigate expected environmental impacts. Therefore, issuance of a Determination of Non-Significance (DNS) is the appropriate threshold determination under the State Environmental Policy Act (SEPA) requirements, with incorporation by reference of:

- The BelRed Corridor Project Draft and Final Environmental Impact Statement, issued July 19, 2007 and addendum.
- Final Environmental Impact Statement, 2016-2027, Transportation Facilities Plan, December 2015.

These documents are available through the Development Services Department, Records Room, Lobby Floor, Bellevue City Hall, 450 110th Avenue NE under file #05-127994-LE.

Adverse impacts which are less than significant are typically subject to City Codes or Standards which are intended to mitigate those impacts. Where such impacts and regulatory items correspond, further documentation is not necessary. For other adverse impacts which are less than significant, Bellevue City Code, Section 22.02.140, provides substantive authority to mitigate impacts disclosed through the environmental review process.

A. NOISE

As conditioned, short term impacts related to noise generation as a result of the construction will be minimized. Noise related to construction is allowed from 7:00 a.m. to 6:00 p.m. Monday through Friday and 9:00 a.m. to 6:00 p.m. on Saturday, except for Federal holidays and as further defined by the Bellevue City Code. Exceptions to the construction noise hours limitation contained in the Noise Control Code MAY be granted pursuant to 9.18.020.C.1 when necessary to accommodate construction which cannot be undertaken during exempt hours. Written requests for exemption from the Noise Control Code must be submitted two weeks prior to the scheduled onset of extended hour construction activity. The use of best available noise abatement technology consistent with feasibility is required during construction to mitigate construction noise impacts to surrounding uses. **Refer to Condition of Approval regarding noise and construction hours and use of best available noise abatement technology in Section XI of this report.**

In addition, per BCC 9.18.045B, noise levels in sleeping areas shall not exceed 40 dBA. The applicant will be required to submit certification by an acoustical consultant

prior to construction of the residential buildings, that the construction methods and materials for the residential units will meet this threshold. If the threshold is exceeded, then noise attenuation measures will be required to be incorporated into the construction design. Once construction is complete, noise levels will be required to be measured inside a random sample of units to ensure sleeping areas do not exceed the 40 dBA threshold. **Refer to Conditions of Approval regarding certification by a noise consultant prior to issuance of building permit and noise levels/measurement in sleeping areas prior to issuance of any certificate of occupancy in Section XI of this report.**

B. AIR QUALITY

During construction, emissions to the air will be released by construction vehicles and heavy equipment. Following construction, emissions from residents' vehicles will be released.

Construction would temporarily increase dust and vehicle emissions near the construction area. Mitigation will include using BMPs to control dust, covering exposed soils, and requiring idling vehicles to be shut off. Construction vehicles will be fitted with required, factory-installed emission control devices. To reduce the potential of dust, construction accesses will be covered with rock or aggregate. Dust emissions will also be reduced during construction through the use of spray water as necessary during dry weather conditions and planting disturbed areas with erosion control seed mix as soon as is practical. Material stockpiles will also be covered or watered as necessary to control dust. These are standard practices imposed on the Clearing & Grading permit. (Bellevue City Code 23.76)

Construction vehicles and heavy construction equipment shall emit the least amount of air pollution as possible. While on city streets, all construction vehicles shall meet the requirements of the Revised Code of Washington 46.61.655 for covered loads. **Refer to Condition of Approval regarding air pollution from construction vehicles and equipment in Section XI of this report.**

C. LIGHT AND GLARE

There is the potential for light and glare with the lights at night from residential units. Potential light and glare impacts will extend to adjacent businesses that operate after dark, in addition to vehicular/pedestrian traffic along adjacent streets. In order to mitigate potential impacts to adjacent vehicular drivers/businesses/residents, any light source emitting from the project area shall incorporate cutoff shields and be designed so as not to provide light and glare and spillover offsite. **Refer to Condition of Approval regarding shielded lights in Section XI of this report.**

D. TRANSPORTATION

Long Term Impacts and Mitigation

The long-term impacts of development projected to occur in the City by 2027 have been addressed in the City's 2016 – 2027 Transportation Facilities Plan FEIS Addendum. The impacts of growth that are projected to occur within the City by 2027 are evaluated on the roadway network assuming that all the transportation improvement projects proposed in the City's 2016 - 2027 Transportation Facilities Plan are in place. The Transportation Facilities Plan EIS divides the City into several Mobility Management Areas (MMAs) for analysis purposes. The project DOL

Townhomes lies within MMA #12, which has a 2027 total growth projection of 113 multifamily dwelling units. This development proposes 31 multi-family dwelling units and demolishing 12,050 square feet of office. Therefore, the volume of proposed development is within the assumptions of the Transportation Facilities Plan FEIS Addendum.

Traffic impact fees are used by the City to fund street improvement projects to alleviate traffic congestion caused by the cumulative impacts of development throughout the City. Payment of the transportation impact fee, as required by BCC 22.16, contributes to the financing of transportation improvement projects in the current adopted Transportation Facilities Plan, and is considered to be adequate mitigation of long-term traffic impacts. Fee payment is required at the time of building permit issuance. Impact fees are subject to change and the fee schedule in effect at the time of building permit issuance will apply. Because the project will result in a trip reduction traffic impact fees will not be required.

Mid-Range Impacts and Mitigation

Project impacts anticipated to occur in the next six years are assessed through a concurrency analysis. The Traffic Standards Code (BCC 14.10) requires that development proposals generating 30 or more new p.m. peak hour trips undergo a traffic impact analysis to determine if the concurrency requirements of the State Growth Management Act are maintained.

The DOL Townhome development will generate a net reduction of 189 p.m. peak hour trips, therefore concurrency is not required for this project.

Short Term Operational Impacts and Mitigation

The applicant's traffic consultant Transpo and City staff analyzed the short term operational impacts of this proposal. The required frontage improvements on NE Bellevue-Redmond Road and 132nd Avenue NE will provide the necessary mitigation for vehicles, pedestrians, and bicycles associated with the project. These improvements include the installation of new sidewalk, planters strips, and a protected bike facility on 132nd Avenue NE to meet the 2009 Pedestrian Bike Plan.

VIII. CHANGES TO PROPOSAL DUE TO CITY REVIEW

Landscaping

- A boardwalk-type sidewalk will be provided along a roughly 80-foot wide section of NE Bellevue-Redmond Road to retain two healthy significant trees that otherwise would have to be removed in order to provide the standard required frontage improvements (sidewalk).
- Mitigation planting of 17 native trees are now provided as replacement for the significant trees that can't be saved along NE Bellevue-Redmond Road and the western and southern property lines, or to otherwise meet landscaping requirements.
- The retaining wall along the western and southern boundaries have been shifted to save and/or improve the survival rate and health of about a dozen significant trees, thereby aiding in retaining the wooded character of the site.
- Additional landscaping is now provided at the development's vehicular entrance to

shield pedestrians and drivers from seeing the internal road, trash bins, and recycling bins, thus improving the overall aesthetic of the neighborhood.

Connectivity/Access

- For pedestrian safety and comfort, the sidewalk along NE Bellevue-Redmond Road was rerouted to provide a landscape buffer between the pedestrian and vehicular traffic on the high-volume road.
- The sidewalk's transitions and curves along NE Bellevue-Redmond Road have been smoothed to allow safer bicycle movements.
- Sidewalks are now strategically located to provide the most convenient connectivity between the site and the larger pedestrian network, including a path towards the future 130th light rail station.
- Relocation of the signal cabinet and signal base to allow for safer and more convenient pedestrian and bicyclist movements.
- Addition of a southbound buffered bicycle lane along 132nd Avenue SE.

Architecture

- Addition on several buildings of strategically placed decorative horizontal cedar slats to avoid large blank walls, improving the design aesthetic of the project.
- A fence has been added at the end of the internal stub road facing NE Bellevue-Redmond Road to shield pedestrians and drivers along NE Bellevue-Redmond Road from seeing the alleyway, trash and recycling bins and improve the overall aesthetic. It is designed and painted to match the adjacent house's faux Cedar siding.

Other

- The multi-family play area has been consolidated, expanded, and shifted away from 132nd Avenue NE to meet Land Use Code requirements.
- A covered facility for secured public bicycle parking was added adjacent central to the project.
- Lighting was added along internal walkways and the play area and bicycle parking for safety and improved functionality.

IX. DECISION CRITERIA

Per Land Use Code 20.30F.145, the Director may approve or approve with modifications an application for Design Review if:

A. The proposal is consistent with the Comprehensive Plan.

Finding: The proposal, as conditioned, is consistent with the Comprehensive Plan and BelRed Subarea Plan. Staff has reviewed the policies which apply to the project and has determined that those applicable policies will be implemented through the application of City Codes and the adopted Design Guidelines identified in the BelRed Chapter of the Land Use Code. **Refer to Conditions of Approval regarding project plans, final landscape plan, building materials/details and color samples, signage, and individual sign permits in Section XI of this report.**

The proposal is consistent with the Comprehensive Plan, including the BelRed Subarea Vision and the following goals and policies from the BelRed Subarea Plan:

Urban Design Goal: *To achieve a design character that results in aesthetically beautiful, distinctive, and long-lasting places that evoke a strong sense of Bellevue and the Northwest, and a dynamic public realm that encourages social interaction.*

- **POLICY S-BR-14.** *Use design guidelines to promote pedestrian-friendly and transit-oriented design, ensure quality and a sense of permanence, promote environmental sustainability, and create a distinct sense of place. Conduct design review for all mixed use, office and residentially designated areas of the Subarea. Apply additional depth and attention to the details of design review within transit-oriented development nodes.*

Discussion: Design review should pay special attention to creating a pedestrian-friendly environment, by helping to create vibrant, interesting, safe, walkable and interconnected sites.

- **POLICY S-BR-16.** *Encourage place-making and a dynamic public realm by integrating publicly accessible plazas, open spaces and other gathering places with development, in public and private projects.*
- **POLICY S-BR-18.** *Encourage diversity in the built environment through a variety of building heights and forms, building articulation and modulation. Encourage building rooflines and floorplates that break down the scale of buildings, help to differentiate Bel-Red from Downtown, and enhance the architectural variety of the area.*
- **Policy S-BR-19.** *Encourage the use of building materials that are of high quality and durability, are appropriate for the area climate, and that have a sense of permanence.*
- **Policy S-BR-20.** *Reinforce the area's sense of place and Northwest provenance by encouraging innovative building and site design, construction techniques and materials that reflect the industrial roots of the area while emphasizing the emerging urban character of Bel-Red.*

Finding:

The Urban Design goals and policies of the BelRed Subarea Plan are incorporated into the Bellevue DOL Townhomes project. The project includes 31 “urban style” townhome units in four 4-unit buildings, three 3-unit buildings, and one 6-unit building. The architectural form and materials are contemporary and provide a simple, clean look with few but highly contrasting colors and materials. The form and materials are a nod to the area’s industrial past but are softened by tastefully incorporating natural materials and earth tones into the design. The proposed central open space will provide space to residents for relaxation. Most perimeter trees will be retained, and proposed landscaping will reflect the Pacific Northwest’s plant life.

Housing Goal: *To encourage Bel-Red redevelopment to result in a diversity of housing types and prices, including a significant share of “workforce housing”.*

- **Policy S-BR-40.** *Encourage a diversity of housing types, from high density, multi-story housing in transit nodes, to medium density housing outside nodes, to other innovative housing forms, such as live/work and work/live units.*

Finding:

The proposal incorporates an urban housing type along the southern edge of the BelRed Subarea which will complement the existing surrounding development to the south, as well as the potential future development to the north, east and west.

B. The proposal complies with the applicable requirements of this Code.

Finding: The proposal complies with all applicable requirements of the Land Use Code. Refer to Section III of this report for specific information on Land Use Code consistency.

C. The proposal addresses all applicable design guidelines or criteria of this Code in a manner which fulfills their purpose and intent.

Finding: As conditioned, the proposal complies with all applicable design guidelines and standards of the Land Use Code. Refer to Section IV of this report for how this proposal has met these standards and guidelines.

D. The proposal is compatible with, and responds to, the existing or intended character, appearance, and quality of development and physical characteristics of the subject property and immediate vicinity.

Finding: As conditioned, the proposed design is compatible and responds to the existing and future character of the BelRed Subarea and adjacent neighborhood.

E. The proposal will be served by adequate public facilities including streets, fire protection, and utilities.

Finding: All urban level public utilities/infrastructure are available to the site or will be constructed/installed as part of this development. All city departments have reviewed the proposal and required associated conditions, as necessary. Refer to section VI of this report for Technical Review.

X. DECISION

After conducting the various administrative reviews associated with the proposal, including applicable Land Use consistency, City Code & Standard compliance reviews and SEPA review, the Director does hereby **APPROVE WITH CONDITIONS** the subject proposal, which includes Design Review and SEPA threshold determination.

XI. CONDITIONS OF APPROVAL:

Compliance with City Codes and Documents

The applicant shall comply with all applicable Bellevue City Codes, Standards, and Ordinances, including, but not limited to the following:

Applicable Codes, Standards and Ordinances

Clearing & Grading Code – BCC 23.76
Construction Codes – BCC Title 23
Fire Code – BCC 23.11
Land Use Code – BCC Title 20

Contact Person

Tom McFarlane, 425-452-5207
Bob Snyder, 425-452-4475
Sean Nichols, 425-452-2926
Faheem Darab, 425-452-2721

Environmental Procedures Code –
BCC Title 22.02
Noise Control – BCC 9.18
Right of Way Use Code – BCC 14.30
Sign Code – BCC Title 22
Transportation Code – BCC 14.60
Utility Code – BCC Title 24

Faheem Darab, 425-452-2721

Faheem Darab, 425-452-2721
Tim Stever, 425-452-4294
Faheem Darab, 425-452-2721
Ryan Miller, 425-452-7915
Lori Santo, 425-452-6828

The following conditions are imposed on the applicant under the authority referenced:

A. GENERAL CONDITIONS: The following conditions apply to all phases of development.

1. Noise & Construction Hours

The proposal will be subject to normal construction hours of 7 a.m. to 6 p.m., Monday through Friday and 9:00 a.m. to 6:00 p.m. on Saturdays, except for Federal holidays and as further defined by the Bellevue City Code. Exceptions to the construction noise hours limitation contained in the Noise Control Code MAY be granted pursuant to 9.18.020.C.1 when necessary to accommodate construction which cannot be undertaken during exempt hours. Written requests for exemption from the Noise Control Code must be submitted **two weeks** prior to the scheduled onset of extended hour construction activity.

AUTHORITY: BCC 9.18.020.C & 9.18.040
REVIEWER: Faheem Darab, Land Use

2. Use of Noise Abatement Technology

The use of best available noise abatement technology consistent with feasibility is required during construction to mitigate construction noise impacts to surrounding uses.

AUTHORITY: BCC 9.18.020F
REVIEWER: Faheem Darab, Land Use

3. Provisions for Loading

The property owner shall provide an off-street loading space which can access a public street. On-street loading and unloading will not be permitted.

AUTHORITY: LUC 20.20.590.K.4 and BCC 14.60.180
REVIEWER: Ryan Miller, Transportation
Faheem Darab, Land Use

4. Utilities

Utility Department approval of the design review application is based on the final conceptual design submitted with this application. Final utility design and construction approval is not given under this permit. Small changes to the site layout may be required to accommodate the utilities after utility engineering is approved. The water, sewer, and storm drainage systems shall be designed per the current City of Bellevue Utility Codes and Utility Engineering Standards. Utilities Department design review, plan approval, and field inspection is performed under the Utility Developer Extension Agreement (UE). A water, sewer and storm Developer Extension Agreement will be required for the project. All connection charges will be due with the Developer Extension

Agreement prior to issuance of the permit. Water, sewer and storm easements will be required.

AUTHORITY: BCC 24.02, 24.04, 24.06
REVIEWER: Lori Santo, Utilities

5. Fire

- a. All access roads in the complex shall be marked as fire lanes in accordance with IFC 503.
- b. An additional fire hydrant may be required within the complex to meet the 400 feet travel distance from a fire hydrant. If the building is equipped with fire sprinklers, that distance can increase to 600'.
- c. Fire sprinkler determination will be made upon the building plans submittal.

AUTHORITY: IFC 503
REVIEWER: Sean Nichols, Fire

6. Addition of Air Conditioning Units

If any changes to the mechanical equipment are proposed, such as addition of air conditioning units exterior to the buildings, the applicant must apply for a mechanical permit. As part of the mechanical permit, the applicant must request a Land Use Exemption, as well as provide screening.

AUTHORITY: LUC 20.20.525
REVIEWER: Faheem Darab, Land Use

7. Modification to the Design Review Plans

Any modification to this approval shall be processed as either a Land Use Exemption or a new Design Review application. Any modification of the project design must be reviewed for consistency with the design intent as stated in Section IX of this report. Conditions of Approval run for the life of the project.

AUTHORITY: LUC 20.30F.175
REVIEWER: Faheem Darab, Land Use

B. PRIOR TO CLEARING & GRADING PERMIT: These conditions must be complied with on plans submitted with the Clearing & Grading or Demolition permit application:

8. Right-of-Way Use Permit

Prior to issuance of any construction or clearing and grading permit, the applicant shall secure applicable right-of-way use permits from the City's Transportation Department, which may include:

- a) Designated truck hauling routes.
- b) Truck loading/unloading activities.
- c) Location of construction fences.
- d) Hours of construction and hauling.
- e) Requirements for leasing of right of way or pedestrian easements.
- f) Provisions for street sweeping, excavation and construction.
- g) Location of construction signing and pedestrian detour routes.
- h) All other construction activities as they affect the public street system.

In addition, the applicant shall submit for review and approval a plan for

providing pedestrian access during construction of this project. Access shall be provided at all times during the construction process, except when specific construction activities such as shoring, foundation work, and construction of frontage improvements prevent access. General materials storage and contractor convenience are not reasons for preventing access.

The applicant shall secure sufficient off-street parking for construction workers before the issuance of a clearing and grading, building, a foundation or demolition permit.

AUTHORITY: BCC 11.70 & 14.30
REVIEWER: Tim Stever, Right-of-Way/Transportation

9. Frontage Improvements and Civil Engineering Plans - Transportation

Civil engineering plans produced by a qualified engineer must be approved by the Transportation Department prior to issuance of the clearing and grading permit. The design of all street frontage improvements and driveway accesses must be in conformance with the requirements of the Americans with Disabilities Act, the Transportation Development Code, the provisions of the Transportation Department Design Manual, and specific requirements stated elsewhere in this document.

All relevant standard drawings from the Transportation Department Design Manual shall be copied exactly into the final engineering plans. Requirements for the engineering plans include, but are not limited to:

- a) Traffic signs and markings.
- b) Curb, gutter, sidewalk, and driveway approach design. The engineering plans shall be the controlling document on the design of these features; architectural and landscape plans must conform to the engineering plans as needed.
- c) Curb ramps, crosswalk revisions, and crosswalk equipment such as pushbuttons. **Reference ADA compliance or provision of a Design Justification Form.**
- d) Installation or relocation of streetlights and related equipment.
- e) Undergrounding of existing overhead utility lines, which should be coordinated with adjacent sites. Transformers and utility vaults to serve the building shall be placed inside the building or below grade, to the extent feasible.
- f) **LANDSCAPING REQUIREMENTS NOTE THAT THE PROPERTY OWNER IS RESPONSIBLE FOR MAINTENANCE UNLESS ACCEPTED BY THE CITY.**
- g) Sight distance. Show the required sight triangles and include any sight obstructions, including those off-site. Sight distance triangles must be shown at all driveway locations and must consider all fixed objects and mature landscape vegetation. Vertical as well as horizontal line of sight must be considered when checking for sight distance.
- h) City standards for driveway widths range from 30 to 36 feet on arterial streets, and 26 to 30 feet for local streets. Driveway aprons must be constructed in accordance with the Transportation Design Manual.
- i) Location of fixed objects in the sidewalk or near the driveway approach.
- j) Trench restoration within any right of way or access easement.

FRONTAGE IMPROVEMENT REQUIREMENTS

1. **NE Bellevue-Redmond Road**

- a. A minimum 8-foot wide concrete sidewalk
(The Transportation Department will allow the construction of a concrete board walk along a portion of the frontage to save a specific tree, If it can be constructed to be ADA compliant. If this boardwalk cannot be constructed to be ADA compliant a concrete sidewalk will be required in its place)
- b. A minimum 20-foot wide landscape planter, the sidewalk may meander through the planter but is not counted towards the planter width requirement.
 - The planter strip shall have street trees 20-ft on-center, shrubs, ground cover, spray irrigation, root barrier, and soil preparation for the street trees.
- c. Street lighting that meets Bellevue Standards.
 - Black poles, arms and fixtures
- d. New concrete curb and gutter shall be installed only at the location of the existing driveway that is to be removed, as required for the reconstructed corner, and at the location of any remaining curb and gutter that is damaged during construction.

2. **132nd Avenue NE**

- a. A minimum 8-foot wide concrete sidewalk
- b. A minimum 18-inch concrete wedge or equivalent for grade separation between the concrete sidewalk and the HMA southbound bike lane.
- c. A minimum 5-foot wide HMA bike lane
- d. A flush vertical curb between the bike facility and the planter, this may be included in the required 5-foot bike lane width.
- e. A minimum 5-foot wide planter
 - The planter strip shall have street trees 20-ft on-center, shrubs, ground cover, spray irrigation, root barrier, and soil preparation for the street trees.
- f. Street lighting that meets Bellevue Standards
 - Black poles, arms and fixtures
- g. A minimum of 17-feet of HMA pavement from the face of curb to the existing centerline of ROW. The use of existing pavement may be used towards meeting this requirement
- h. Transitions for the sidewalk and protected bike lane back into the existing street section to the south
- i. Channelization and signage within the road and behind the curb, within the protected southbound bike lane and sidewalk.
- j. New concrete curb and gutter

3. **Intersection of 132nd Avenue NE and NE Bellevue-Redmond Road (The SW Corner)**

- a. Reconstruct the corner with new signal equipment and an ADA compliant ramp that accommodates the N/S protected bike lane. The preliminary ramp design will require radial cast iron domes with a thickened concrete section where vehicles may track onto the truncated domes.

- The City shall provide the developer the following signal components: Terminal Cans, Type 1 Poles, Type PS poles, Type III signal Pole with Mast Arm with Anchor Bolts, Signal Heads, Pedestrian Clams, D-Mount, the Luminaire, APS Push Buttons, and APS CCU if needed. The labor, signal pole foundation, and any other materials not specifically listed above is the responsibility of the developer. If the City does not provide the specific materials listed above the requirement to install new signal equipment at the corner will be waived and the current signal equipment will remain as is. This waiver does not apply to ADA or the need to reconstruct the corner to meet ADA to the maximum extent feasible.
- b. Street lighting that meets Bellevue Standards
 - Black poles, arms and fixtures
- c. Install a new signal pole/mast arm with ADA compliant push buttons, pedestrian heads.

4. ADA:

The Americans with Disabilities Act (ADA) requires that sidewalk cross slopes not exceed two percent. The sidewalk cross slope may be less than two percent only if the sidewalk has a longitudinal slope sufficient to provide adequate drainage. Bellevue's standard for curb height is six inches, except where curb ramps are needed. The engineering plans must comply with these requirements, and must show adequate details, including spot elevations, to confirm compliance. New curb and sidewalk shall be constructed in compliance with these requirements. Building elevations shall be consistent with the required curb and sidewalk elevations. Spot elevations must be included in the building plans in a manner that proves that building elevations are designed to correspond to the sidewalk elevations shown in the engineering plans, especially at entrances and other key points. Curb and sidewalk elevations will not be revised to fit the building, and city inspectors may require spot surveys during construction in order to confirm the required elevations.

ADA also requires provision of a safe travel path for visually handicapped pedestrians. Potential tripping hazards are not allowed in the main pathway. Any planter boxes installed in the sidewalk to improve pedestrian sight distance at driveways must be designed to reduce the tripping potential and must not extend more than two feet into the public sidewalk. Traffic signal controller boxes and streetlight contactor cabinets must be located so as not to interfere with the main pedestrian path. Buildings shall be designed so that doors do not swing out into the pedestrian path. Installation of colored or textured bands to guide pedestrians in the direction of travel is advisable, subject to the requirements for non-standard sidewalk features. ADA-compliant curb ramps shall be installed where needed, consistent with City and WSDOT standard drawings. If such standards cannot be met, then deviation from standards must be justified on a Design Justification Form to be filed with the Transportation Department.

5. To the extent feasible, no new utility vaults that serve only one development will be allowed within a public sidewalk. Vaults serving a broader public purpose may be located within a public sidewalk. To the extent feasible, no utility vaults may be located within the primary walking path in any sidewalk. All existing vault lids shall be replaced to be non-slip.
6. No fixed objects, including fire hydrants, trees, and streetlight poles, are allowed within ten feet of a driveway edge, defined as Point A in standard drawing SW-140-1 or equivalent. Fixed objects are defined as anything with breakaway characteristics greater than a four-inch by four-inch wooden post.
7. No new overhead utility lines will be allowed within or across any right of way or sidewalk easement, and existing overhead lines must be relocated underground.
8. A private road street name sign is required to be installed. The applicant has the option of paying the City to manufacture the street name sign or having one manufactured by a private vendor. The applicant is responsible for procuring the sign post and hardware and is responsible for installation. The specifics of the street name sign will be determined during the clearing and grading permit.
9. The applicant is required to coordinate mailbox location with the Bellevue Postmaster and show the mailbox location on the engineering plans within the site.

Construction of all street and street frontage improvements must be completed prior to any form of occupancy, including any form of temporary or phased occupancy. A Design Justification Form must be provided to the Transportation Department for any aspect of any pedestrian route adjacent to or across any street that cannot feasibly be made to comply with ADA standards. Design Justification Forms must be provided prior to approval of the clear and grade plans for any deviations from standards that are known in advance. Forms provided in advance may need to be updated prior to project completion. For any deviations from standards that are not known in advance, Forms must be provided prior to project completion.

AUTHORITY: BCC 14.60; Transportation Department Design Manual;
Americans with Disabilities Act
REVIEWER: Ryan Miller, Transportation

10. Air Pollution from Construction Vehicles and Equipment

Construction vehicles and heavy construction equipment shall emit the least amount of air pollution as possible. While on city streets, all construction vehicles shall meet the requirements of the Revised Code of Washington 46.61.655 for covered loads.

AUTHORITY: State Environmental Policy Act, Bellevue City Code, 23.76,

REVIEWER: Revised Code of Washington 46.61.655
Faheem Darab, Land Use

11. Boardwalk

The boardwalk along NE Bellevue-Redmond Road must conform to the design details included in the approved plans. This includes a minimum 8-foot wide clear width as well as ADA accessible design. As the boardwalk design develops further and proceeds to construction, the design and construction must minimize disruption of the tree roots. The City and the applicant's arborist must be consulted. In addition, the applicant shall provide the City with an additional ten PermaTrak precast concrete treads and a 30-foot length of 1/8" rubber leveling pad for future maintenance purposes.

If an ADA compliant design is found by the City to be impossible, an 8-foot wide, ADA compliant concrete sidewalk shall be constructed in its place and trees #746 and #744 may be removed and replaced with two evergreen trees of at least 2.5-inch caliper each.

AUTHORITY: LUC 20.25D.150
REVIEWER: Faheem Darab, Land Use

12. Bicycle Parking

The applicant shall provide a covered bicycle parking facility for the public's use with room for at least 4 bicycles in accordance with the BelRed Land Use Code 20.25D.120.G.

AUTHORITY: LUC 20.25D.120
REVIEWER: Faheem Darab, Land Use

13. Final Landscape Plan

The final Landscape Plan shall be submitted and reviewed as part of the Clearing and Grading Permit application for review by Land Use. Provide details for any electrical outlets that are proposed to be located within the planting strips.

AUTHORITY: LUC 20.20.520
REVIEWER: Faheem Darab, Land Use

14. Street Trees and Right-of-Way Landscaping

All street trees along NE Bellevue-Redmond Road and 132nd Avenue NE shall be planted as specified by Parks Department tree planting standards. The species to be planted are as follows:

Pseudotsuga Menziesii 'Douglas Fir': 10' in height at planting
Quercus Frainetto 'Forest Green Oak': 2.5" caliper at planting

AUTHORITY: LUC 20.25D.110.B.3.c. and 20.25D.110.B.3.e
REVIEWER: Faheem Darab, Land Use

15. Tree Protection

Include a Tree Protection Plan with the project plans submitted for construction permits and include Tree Preservation Detail Drawing #35 and the City of Bellevue Development Standards.

AUTHORITY: BCC 23.76 and Bellevue Development Standards
REVIEWER: Faheem Darab, Land Use

16. Tree Mitigation Planting

As shown on the Landscape Plan (sheet L-300) the applicant shall install all 17 evergreen trees along, or just inside the perimeter of the north, west and south. These trees shall be shown on the Final Landscape Plan, submitted as part of the Clearing and Grading Permit.

AUTHORITY: LUC 20.25D.110.B and C and D
REVIEWER: Faheem Darab, Land Use

C. PRIOR TO ISSUANCE OF BUILDING PERMIT: Unless specified otherwise below, these conditions must be complied with on plans submitted with the Building Permit Application:

17. Compliance with Submittal Requirements

Compliance with the provisions and submittal requirements of RCW 64.55 shall be reviewed by the Building Department prior to issuance of a permit for construction.

AUTHORITY: RCW 64.55
REVIEWER: Bob Snyder, Building Division

18. Accessible Route

At least one accessible route is required to be provided from public transportation stops, accessible parking, accessible passenger loading zones, and public streets or sidewalks to the accessible building entrances served. Building permit drawings are required to demonstrate that these requirements will be met.

AUTHORITY: IBC 1104.1
REVIEWER: Bob Snyder, Building Division

19. Private Parking

Parking within the footprint of the townhouse dwelling unit (garage) is limited to the private parking for that unit only.

AUTHORITY: IRC 302.0
REVIEWER: Bob Snyder, Building Division

20. Building Materials/Details and Color Samples

The development is subject to the building materials/details and color samples as shown on the attached plans.

AUTHORITY: LUC 20.25D.150.D
REVIEWER: Faheem Darab, Land Use

21. Solid Waste/Recycling Collection Facilities

Prior to the issuance of any construction permit, the applicant shall provide a written document demonstrating that Republic Services (City's waste hauler service) has been contacted and has approved the proposed access to the

recycling and solid waste collection areas for this development. In addition, all pick-up functions will be handled on-site within the internal access road and all rights-of-way and public easements shall not be occupied by trash receptacles, dumpsters, recycling bins or other such items. Trash and recycling bins will be stored within each residential unit's garage. Bins will be placed outside garages on pick-up day, serviced, and replaced back within garages.

AUTHORITY: LUC 20.20.725
REVIEWER: Faheem Darab, Land Use

22. Shielded Lights

In order to mitigate for potential impacts to adjacent properties, including other residents within the development, any light source emitting from the project area shall be designed so as not to provide light and glare and spillover offsite. Cutoff shields shall be used.

AUTHORITY: Land Use Code 20.20.522
REVIEWER: Faheem Darab, Land Use

23. Bicycle Parking

The applicant shall provide a minimum of 4 covered bicycle parking stalls for public use in accordance with the BelRed Land Use Code 20.25D.120.G and as shown in the attached plans.

AUTHORITY: Land Use Code 20.25D.120
REVIEWER: Faheem Darab, Land Use

24. Building and Site Plans – Transportation

The building grade and elevations shall be consistent with the curb and sidewalk grade shown in the approved civil engineering plans. During construction, city inspectors may require additional survey work at any time in order to confirm proper elevations. Building plans, landscaping plans, and architectural site plans must accommodate on-site traffic markings and signs and driveway design as specified in the engineering plans. Building plans, landscaping plans, and architectural site plans must comply with vehicle and pedestrian sight distance requirements, as shown on the engineering plans.

AUTHORITY: BCC 14.60.060, 110, 120, 150, 180, 181, 190, 240, 241
REVIEWER: Ryan Miller, Transportation

25. Existing Easements

Any utility easements contained on this site which are affected by this development must be identified. Any negative impact that this development has on those easements must be mitigated or easements relinquished.

AUTHORITY: BCC 14.60.100
REVIEWER: Ryan Miller, Transportation

26. Easements for Signal Control and Street Light Boxes and Vaults

The applicant shall provide easements to the City for location of signal and street light facilities such as above-grade boxes and below-grade vaults between the building and sidewalk within the landscape area.

AUTHORITY: BCC 14.60.100
REVIEWER: Ryan Miller, Transportation

27. Sidewalk/Utility Easements

The applicant shall provide sidewalk and utility easements to the City such that the required frontage improvements are located within a pedestrian easement area.

AUTHORITY: BCC 14.60.100
Reviewer: Ryan Miller, Transportation

28. Dedication of Right-of-Way

The applicant shall dedicate right of way to the City along the property frontage such that street improvements to the back of curb are located within the public right of way.

AUTHORITY: BCC 14.60.090
REVIEWER: Ryan Miller, Transportation

D. PRIOR TO ISSUANCE OF ANY CERTIFICATE OF OCCUPANCY: The following conditions are required by City Code and supported by City Policy and shall be complied with prior to issuance of the Temporary Certificate of Occupancy (TCO):

29. Addressing of Buildings

The applicant shall contact Jami Fairleigh, Information Technology Department, regarding the final addressing of each building unit. Addressing shall be approved by the Fire Department and Transportation Department.

AUTHORITY: Uniform Fire Code 505
REVIEWER: Jami Fairleigh, Information Technology

30. Street Frontage Improvements

All street frontage improvements and other required transportation elements, including street light and traffic signal revisions, must be constructed by the applicant and accepted by the Transportation Department inspector. All existing street light and traffic signal apparatus affected by this development, including traffic controllers, pedestrian signal poles, traffic signal poles, and power sources, must be relocated as necessary. Existing overhead lines must be relocated underground. All required improvements must be constructed as per the approved plans or as per direction of the Transportation Department inspector. Bonding or other types of assurance devices will not be accepted in lieu of construction, unless the City requires a delay.

AUTHORITY: BCC 14.60; Comprehensive Plan Policy UT-39;
Transportation Department Design Manual, and
Transportation Design Manual Standard Drawings
REVIEWER: Ryan Miller, Transportation

31. Pavement Restoration

Pavement restoration associated with street frontage improvements or to repair damaged street surfaces shall be provided as follows:

Near this project, Bellevue Redmond Road is classified as Standard Trench Restoration and 132nd Avenue NE is classified as Grind and Overlay Restoration.

AUTHORITY: BCC 14.60. 250; Design Manual Design Standard #23
REVIEWER: Tim Stever, Right-of-Way/Transportation

32. Landscape Installation Assurance Device

All site landscaping shall be 100% complete per the plan approved by the City. Alternatively, the applicant shall submit the following: 1) a red-marked plan identifying which landscape areas are incomplete; 2) an estimate for the total cost to complete these areas; and 3) a notarized Assignment of Savings dedicated to the City for 150% of the estimated cost to complete these areas per the approved Landscape Plan. The assurance device will be released upon complete installation and inspection approval by Land Use and Clearing and Grading.

AUTHORITY: LUC 20.40.490
REVIEWER: Faheem Darab, Land Use

33. Landscape Maintenance Assurance Device

After final inspection of the landscape installation by Land Use and the Clearing and Grading inspector, a landscape maintenance assurance device must be filed with the Development Services Department for a one-year period in the form of an assignment of savings or letter of credit for 20% of the cost of all labor and materials for all required landscaping including the right-of-way.

AUTHORITY: LUC 20.40.490
REVIEWER: Faheem Darab, Land Use

34. Noise Levels/Measurements in Sleeping Areas

Noise levels in sleeping areas shall not exceed 40 dBA. The applicant shall measure the noise levels in a random sample of the rooms and submit the findings to the City. If the maximum threshold is exceeded, additional noise mitigation will be required to meet the threshold prior to the issuance of an Occupancy Permit.

AUTHORITY: Bellevue City Code 9.18.045B and LUC 20.30F.145
REVIEWER: Faheem Darab, Land Use

E. PRIOR TO ISSUANCE OF SIGN PERMIT(S)

35. Signage and Individual Sign Permits

There are no implied approvals of proposed signage within this Design Review approval. comprehensive sign package shall be submitted to the Development Services Department for review and approval through a Pre-Development Services (DC) application or a Sign Permit (SA) application. Contents shall include but is not limited to:

- Location

- Lighting
- Color Palate
- Materials
- Design Concept

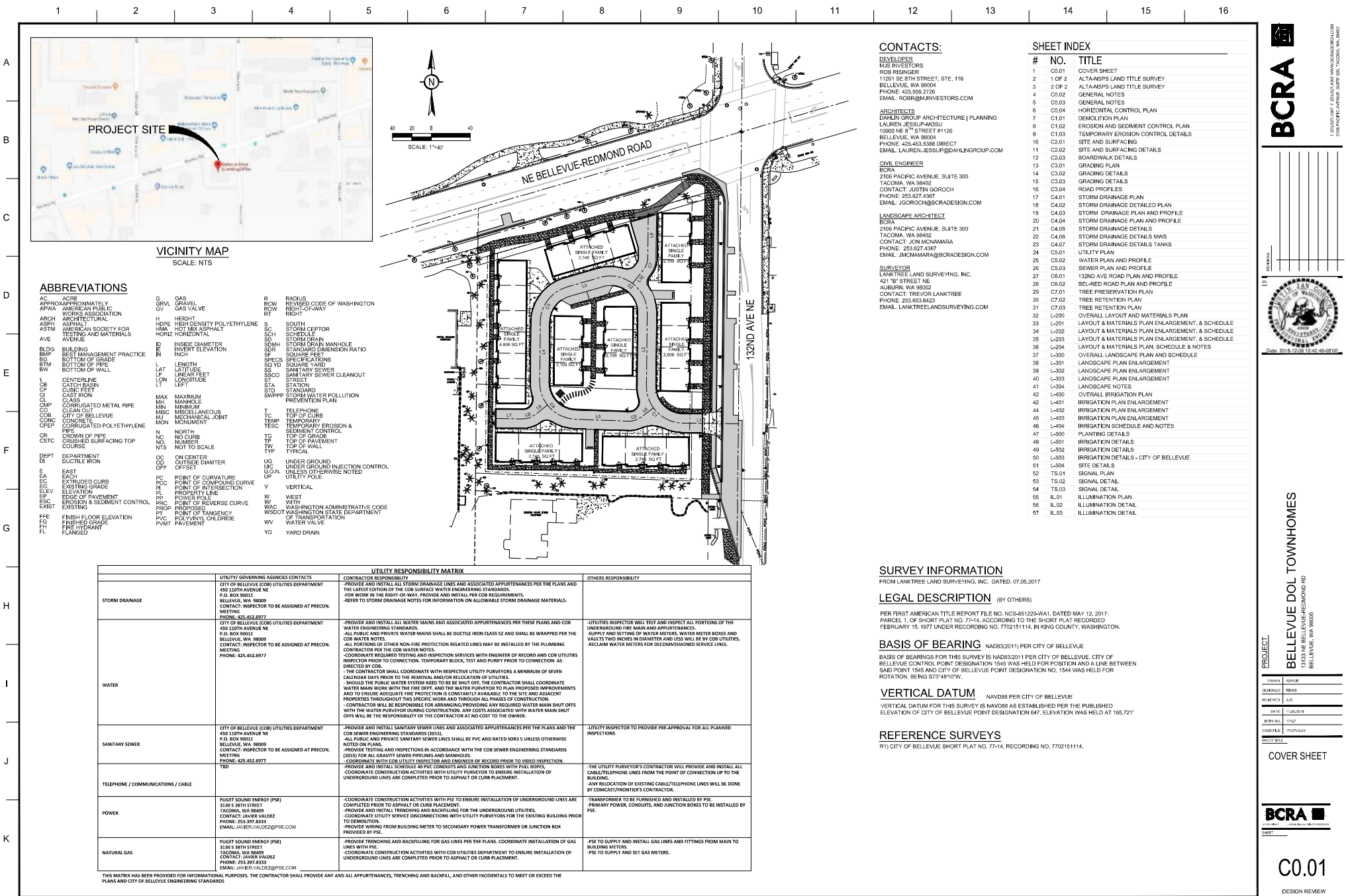
Signage shall be the minimum necessary to convey information and shall be architecturally compatible and integrated with the building. Maximum letter and sign size shall be determined based on proposed location and other design criteria. Sign Code limitations are not a guaranteed maximum and cannot be exceeded. Final design review of individual signs and compliance with the approved sign package will occur through review of each sign permit application.

No freestanding signs, cabinet signs or signs above the rooftop are permitted except as permitted in the Sign Code Bellevue City Code 22B.10

AUTHORITY: Bellevue City Code 22B.10 and LUC 20.25D.150.F
REVIEWER: Faheem Darab, Land Use

Attachments

- A. Project Plans
- B. Determination of Non-Significance
- C. SEPA Checklist





ALTA/NSPS LAND TITLE SURVEY

LEGAL DESCRIPTION:
PER FIRST AMERICAN TITLE REPORT FILE NO. NCS-851220-WA1, DATED MAY 12, 2017.

PARCEL 1, OF SHORT PLAT NO. 77-14, ACCORDING TO THE SHORT PLAT
RECORDED FEBRUARY 15, 1977
UNDER RECORDING NO. 7702151114, IN KING COUNTY, WASHINGTON.

BASIS OF BEARINGS NAD83(2011) DATUM
BASIS OF BEARINGS FOR THIS SURVEY IS NAD83(2011) PER CITY OF BELLEVUE.
CITY OF BELLEVUE CONTROL POINT DESIGNATION 1545 WAS HELD FOR POSITION
AND A LINE BETWEEN SAID POINT 1545 AND CITY OF BELLEVUE POINT
DESIGNATION NO. 1544 WAS HELD FOR ROTATION, BEING S73°48'10"W.

VERTICAL DATUM
VERTICAL DATUM FOR THIS SURVEY IS NAVD88 AS ESTABLISHED PER THE
PUBLISHED ELEVATION OF CITY OF BELLEVUE POINT DESIGNATION 647.
ELEVATION WAS HELD AT 165.721'

SITE ADDRESS
13133 NE BELLEVUE-REDMOND ROAD, BELLEVUE WA 98005

ASSESSOR'S PARCEL NUMBER
2825059084

FLOOD INFORMATION
PARCEL IS LOCATED IN ZONE "X" - AREAS DETERMINED TO BE OUTSIDE THE
500-YEAR FLOODPLAIN.
PER FEMA PANEL NUMBER 53033C0656F, DATED MAY 16, 1995.

PARCEL AREA
71,509 SQ FT

REFERENCE SURVEYS
R1) CITY OF BELLEVUE SHORT PLAT NO. 77-14, RECORDING NO. 7702151114.

NOTES

1. ALL DISTANCES SHOWN ON THIS SURVEY ARE IN US SURVEY FOOT.

2. UTILITIES ARE SHOWN PER SURFACE OBSERVATIONS, UTILITY PAINT MARKINGS
PER PRIVATE UTILITY LOCATOR ENLISTED AS PART OF THIS SURVEY AND OTHER
AVAILABLE DATA. POTHOLES SHOULD BE PERFORMED TO DETERMINE EXACT
LOCATIONS AND DEPTHS OF UTILITIES SHOWN HEREON OR OTHER
NON-CONDUCTIBLE UTILITIES.

3. FIELD SURVEY WAS COMPLETED IN JUNE 2017 AND ALL MONUMENTS SHOWN
AS FOUND WERE VISITED AT THIS TIME OR AS NOTED.

4. THIS SURVEY IS A RETRACEMENT OF THE DEEDED DESCRIPTION STATED ABOVE
AND DOES NOT PURPORT TO SHOW ANY UNRECORDED OWNERSHIP RIGHTS.

5. NO EVIDENCE OF RECENT EARTH MOVING WORK, BUILDING CONSTRUCTION, OR
BUILDING ADDITIONS WERE OBSERVED IN THE PROCESS OF CONDUCTING THE
FIELDWORK.

6. THERE ARE A TOTAL OF 86 PARKING SPACES, 82 REGULAR AND 4 HANDICAP.

7. NO ZONING REPORT PROVIDED AS PART OF THIS SURVEY.

PROCEDURE / NARRATIVE:
A FIELD TRAVERSE USING A TRIMBLE S6 TOTAL STATION SUPPLEMENTED WITH
FIELD NOTES AND TRIMBLE R6 MODEL 4 NETWORK RTK GPS ROVER, WAS
PERFORMED, ESTABLISHING THE ANGULAR, DISTANCE, AND VERTICAL
RELATIONSHIPS BETWEEN THE MONUMENTS, PROPERTY LINES AND AND
IMPROVEMENTS. THE RESULTING DATA MEETS OR EXCEEDS THE STANDARDS
FOR LAND BOUNDARY SURVEYS AS SET FORTH IN WAC 332-130-090.

SPECIAL EXCEPTIONS:
PER FIRST AMERICAN TITLE REPORT FILE NO. NCS-851220-WA1, DATED
JUNE 9, 2017:

ITEMS 1-2) ITEMS REGARDING TAXES AND FISCAL RESPONSIBILITY.
(NON SURVEY RELATED ITEMS)

ITEM 3) ITEM REGARDING KING COUNTY TREATMENT CAPACITY CHARGE, AS
AUTHORIZED UNDER ROW 35.58 & KING COUNTY CODE 28.84.050.
(NON SURVEY RELATED ITEM)

ITEM 4) FACILITY CHARGES, IF ANY, INCLUDING BUT NOT LIMITED TO
HOOK-UP, OR CONNECTION CHARGES AND LATECOMER CHARGES FOR WATER
OR SEWER FACILITIES OF CITY OF BELLEVUE AS DISCLOSED BY INSTRUMENT
RECORDED NOVEMBER 09, 1977, DECEMBER 20, 1996 AND DECEMBER 22,
2011 AS RECORDING NOS. 7711090948, 9612200938 AND
20111222000589. (NON SURVEY RELATED ITEM)

ITEM 5) A LEASE DATED UNDISCLOSED, EXECUTED BY 13133 BEL-RED ROAD,
LLC, A LIMITED LIABILITY COMPANY, FOR ITS HEIRS, EXECUTORS,
ADMINISTRATORS, SUCCESSORS, AND ASSIGNS AS LESSOR AND THE STATE OF
WASHINGTON, DEPARTMENT OF LICENSING ACTING THROUGH THE DEPARTMENT
OF ENTERPRISE SERVICES AS LESSEE, FOR A TERM OF OCTOBER 1, 2014
AND ENDING SEPTEMBER 30, 2019 RECORDED OCTOBER 17, 2013 AS
RECORDING NO. 20131017001362 OF OFFICIAL RECORDS. SAID LEASE,
AMONG OTHER THINGS PROVIDES FOR: THE LEASE MAY, AT THE OPTION OF
THE LESSEE, BE RENEGOTIATED FOR AN ADDITIONAL FIVE (5) YEARS.
(NON SURVEY RELATED ITEM)

ITEM 6) RIGHT TO MAKE NECESSARY SLOPES FOR CUTS OR FILLS UPON
SAID PREMISES FOR ROAD AS GRANTED BY DEED RECORDED DECEMBER 16,
1943. UNDER RECORDING NO. 3355043.
(BLANKET IN NATURE AFFECTING PROPERTY ALONG THE WEST MARGIN OF
132ND AVENUE SOUTHEAST- NOT PLOTTABLE)

ITEM 7) EASEMENT, INCLUDING TERMS AND PROVISIONS CONTAINED THEREIN:
RECORDING DATE: JANUARY 26, 1973
RECORDING INFORMATION: 7301260478
IN FAVOR OF: CITY OF BELLEVUE
FOR: LANDSCAPING, SIDEWALKS, TRAILS, STREET LIGHTING, TRAFFIC SIGNAL
EQUIPMENT, WITH NECESSARY APPURTENANCES AND SLOPE RETAINING
STRUCTURES OR NECESSARY CUT AND FILL SLOPES.
(AS SHOWN ALONG THE SOUTH MARGIN OF BELLEVUE-REDMOND ROAD,
NORTHERLY 20' OF PROPERTY)

ITEM 8) THE TERMS AND PROVISIONS CONTAINED IN THE DOCUMENT ENTITLED
"CONCOMITANT ZONING AGREEMENT" RECORDED JANUARY 18, 1977 AS
RECORDING NO. 7701180812 OF OFFICIAL RECORDS. DOCUMENT(S)
DECLARING MODIFICATIONS THEREOF RECORDED NOVEMBER 23, 1977 AS
RECORDING NO. 7711231142 OF OFFICIAL RECORDS. (AGREEMENT CREATING
RESTRICTIONS AND PROVISIONS OF USES OVER THE SOUTH 110'± OF THE
PROPERTY. RESTRICTIONS ON THE ENTIRE AGREEMENT AREA FOR USE FOR
PARKING, REQUIREMENTS OF ENTRY TO BEL-RED ROAD AND 132ND AVENUE
NE, AND LIGHTING. DOCUMENTS ALSO CONTAIN PROVISIONS FOR THE
PLACEMENT OF LANDSCAPE SCREENING ALONG SOUTH AND EAST PROPERTY
LINES. AREA OF AGREEMENT AND LANDSCAPE SCREENING SHOWN HEREON.)

SPECIAL EXCEPTIONS (CONTINUED):
ITEM 9) RESTRICTIONS, CONDITIONS, DEDICATIONS, NOTES, EASEMENTS AND
PROVISIONS, IF ANY, AS CONTAINED AND/OR DELINEATED ON THE FACE OF
THE SHORT PLAT NO. 77-14 RECORDED FEBRUARY 15, 1977 AS RECORDING
NO. 7702151114, IN KING COUNTY, WASHINGTON.
(SHOWN HEREON- INCLUDES 20' EASEMENT FOR UNDEFINED PURPOSE AND
DEDICATION OF PORTION DEDICATED IN NORTHEAST CORNER OF PROPERTY)

ITEM 10) EASEMENT, INCLUDING TERMS AND PROVISIONS CONTAINED THEREIN:
RECORDING DATE: MARCH 07, 1977
RECORDING INFORMATION: 7703070941
IN FAVOR OF: PUGET SOUND ENERGY, INC., A WASHINGTON CORPORATION
FOR: ELECTRIC AND/OR GAS TRANSMISSION AND/OR DISTRIBUTION SYSTEM
(SHOWN HEREON, NEAR THE NORTHEAST CORNER OF THE BUILDING)

ITEM 11) MAINTENANCE AGREEMENT WITH AMERICAN BUILDING MAINTENANCE
COMPANY DATED JUNE 15, 1997 AS DISCLOSED BY WARRANTY DEED
RECORDED AUGUST 07, 1980 UNDER RECORDING NUMBER 8008070431.
(NON SURVEY RELATED ITEM)

ITEM 12) DEED OF TRUST AND THE TERMS AND CONDITIONS THEREOF.
GRANTOR/TRUSTOR: 13133 BEL-RED ROAD LLC, A WASHINGTON LIMITED
LIABILITY COMPANY
GRANTEE/BENEFICIARY: WASHINGTON TRUST BANK
TRUSTEE: UPT WASHINGTON, INCORPORATED
RECORDED: MARCH 23, 2016
RECORDING INFORMATION: 20160323000450

ITEMS 13-19 TITLE COMPANY CONCERNS. (NON SURVEY RELATED ITEMS)

TO:
CEDAR COAST CAPITAL CORP., AND FIRST AMERICAN TITLE INSURANCE COMPANY.

THIS IS TO CERTIFY THAT THIS MAP OR PLAN AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH
THE 2016 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/NSPS LAND TITLE SURVEYS, AS ESTABLISHED AND
ADOPTED BY KSA AND NSPS, AND INSUREES (ENGINEERS, SURVEYORS, AND LAND SURVEYORS) SHALL BE HELD RESPONSIBLE
THEREOF. THE FIELD WORK WAS COMPLETED ON JUNE 05, 2017.

DATE OF PLAN OR MAP: JULY 6, 2017

[Signature]
JAMES H. HETTINGER P.L.L.C.
WASHINGTON REGISTRATION NO. 41282

7/6/2017
DATE



Job Number
2459

Sheet
1 of 2



LAN'TREE
LAND SURVEYING, INC.
421 1st STREET N.E. AUBURN, WA 98002
PHONE: (206) 835-4223
FAX: (206) 783-1616
WWW.LAN'TREELANDSURVEYING.COM

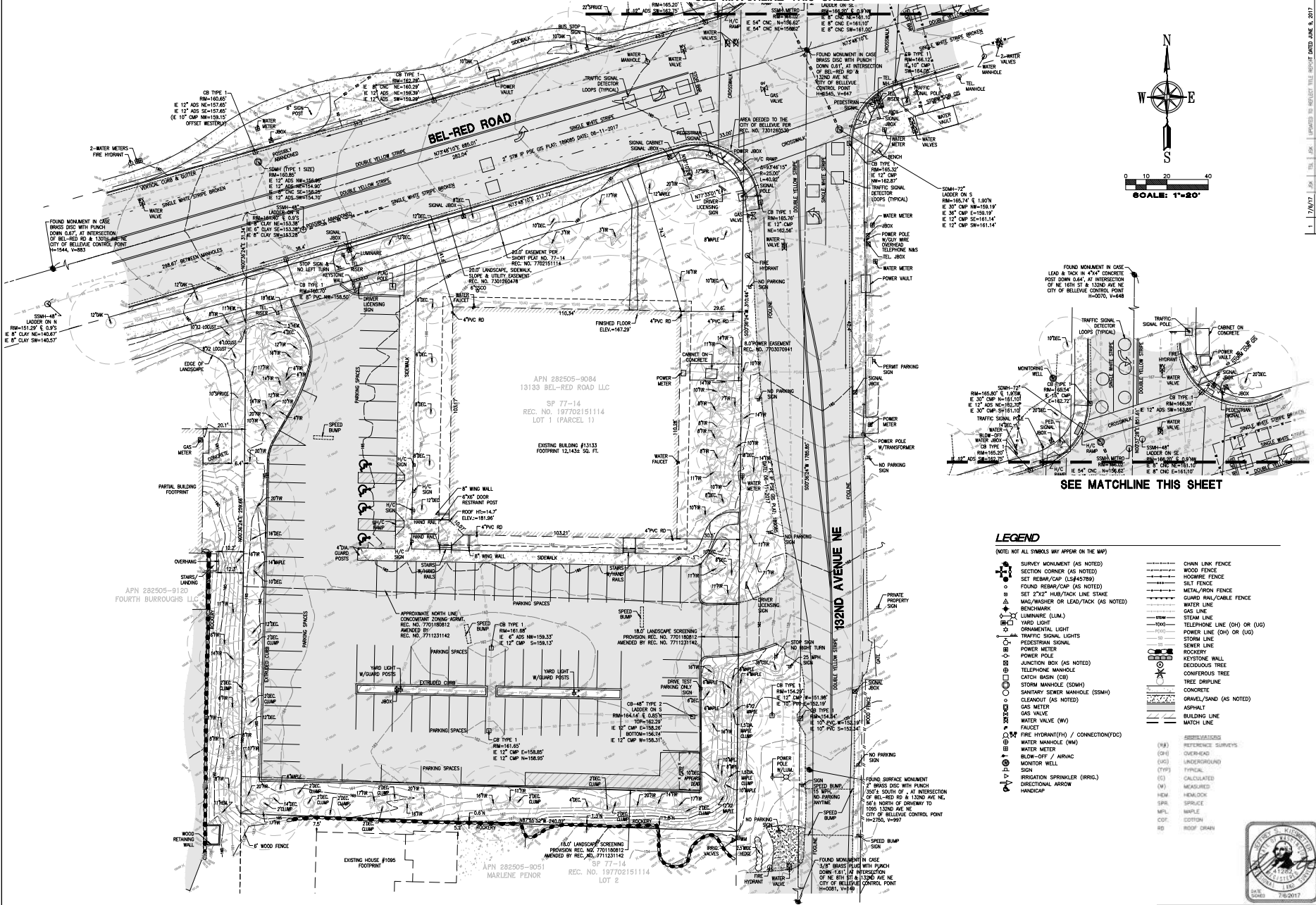
Scale:
Horizontal N/A
Vertical N/A
Designed: N/A
Drawn: JHL
Checked: JHL
Approved: JHL
Date: 7/6/17

For:
CEDAR COAST CAPITAL CORP
1021 WEST HASTINGS STREET, SUITE 1200
VANCOUVER, BC V6E 0C3

Title:
ALTA/NSPS LAND TITLE SURVEY
PTN OF THE SE1/4 OF SEC. 28,
TWP. 28 N., RGE 6 EAST, W. M.
CITY OF BELLEVUE
KING COUNTY STATE OF WASHINGTON

ALTA/NSPS LAND TITLE SURVEY

SEE MATCHLINE THIS SHEET



SEE MATCHLINE THIS SHEET

LEGEND

(NOTE: NOT ALL SYMBOLS MAY APPEAR ON THE MAP)

- | | |
|--|--|
| <ul style="list-style-type: none"> ○ SURVEY MONUMENT (AS NOTED) □ SECTION CORNER (AS NOTED) □ SET REBAR/CAP (L345789) ○ FOUND REBAR/CAP (AS NOTED) ○ SET 2"x2" HUB/TACK LINE STAKE △ MAG/WASHER OR LEAD/TACK (AS NOTED) ○ BENCHMARK ○ LUMINAIRE (LUM.) ○ VARIO LIGHT ○ ORNAMENTAL LIGHT ○ TRAFFIC SIGNAL LIGHTS ○ PEDESTRIAN SIGNAL ○ POWER METER ○ POWER POLE ○ JUNCTION BOX (AS NOTED) ○ TELEPHONE MANHOLE ○ CATCH BASIN (CB) ○ STORM MANHOLE (SMH) ○ SANITARY SEWER MANHOLE (SSMH) ○ CLEANOUT (AS NOTED) ○ GAS METER ○ GAS VALVE ○ WATER VALVE (WV) ○ FAUCET ○ FIRE HYDRANT (FH) / CONNECTION (FC) ○ WATER MANHOLE (WM) ○ BLOW-OFF / AIR/VAC ○ MONITOR WELL ○ IRRIGATION SPRINKLER (IRRG) ○ DIRECTIONAL ARROW ○ HANDICAP | <ul style="list-style-type: none"> — CHAIN LINK FENCE — WOOD FENCE — HOOFER FENCE — SILT FENCE — METAL/IRON FENCE — GUARD RAIL/CABLE FENCE — WATER LINE — GAS LINE — TELEPHONE LINE (OH) OR (UG) — POWER LINE (OH) OR (UG) — STORM LINE — SEWER LINE — ROCKERY — KEYSTONE WALL — DECIDUOUS TREE — CONIFEROUS TREE — TREE DRUPLINE — CONCRETE — GRUVEL/SAND (AS NOTED) — ASPHALT — BUILDING LINE — MATCH LINE |
|--|--|

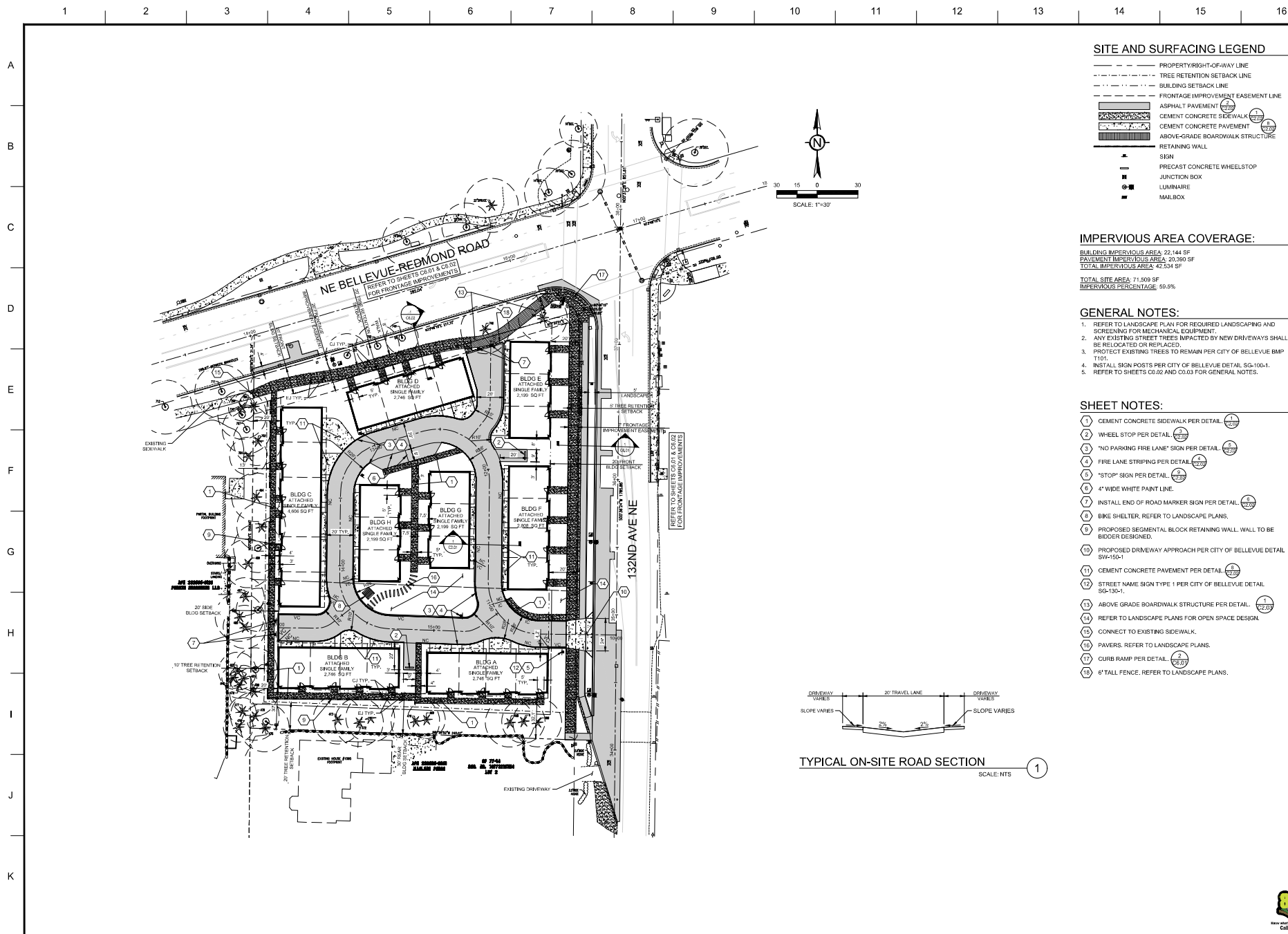
ALTA/NSPS LAND TITLE SURVEY
PTN OF THE SE1/4 OF SEC. 26,
TWP. 26 N., RGE. 6 EAST, W. 4.
CITY OF BELLEVUE
KING COUNTY STATE OF WASHINGTON

For: CEDAR COAST CAPITAL CORP
1021 WEST HASTINGS STREET, SUITE 1200
VANCOUVER, BC V6E 0C3

Scale: Horizontal 1"=40' Vertical 1"=10'
Designed: JVA
Drawn: JBL
Checked: JBL
Date: 7/26/17

LAN'TREE LAND SURVEYING, INC.
4711 15th Street NE, Auburn, WA 98002
Phone: (206) 835-4243
Fax: (206) 763-1616
WWW.LANTREELANDSURVEYING.COM

Job Number: 2459
Sheet: 2 of 2
Date: 7/26/2017



SITE AND SURFACING LEGEND

	PROPERTY RIGHT-OF-WAY LINE
	TREE RETENTION SETBACK LINE
	BUILDING SETBACK LINE
	FRONTAGE IMPROVEMENT EASEMENT LINE
	ASPHALT PAVEMENT
	CEMENT CONCRETE SIDEWALK
	CEMENT CONCRETE PAVEMENT
	ABOVE-GRADE BOARDWALK STRUCTURE
	RETAINING WALL
	SIGN
	PRECAST CONCRETE WHEELSTOP
	JUNCTION BOX
	LUMINAIRE
	MAILBOX

IMPERVIOUS AREA COVERAGE:

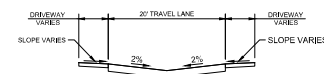
BUILDING IMPERVIOUS AREA: 22,144 SF
PAVEMENT IMPERVIOUS AREA: 20,980 SF
TOTAL IMPERVIOUS AREA: 43,124 SF
TOTAL SITE AREA: 71,309 SF
IMPERVIOUS PERCENTAGE: 60.5%

GENERAL NOTES:

1. REFER TO LANDSCAPE PLAN FOR REQUIRED LANDSCAPING AND SCREENING FOR MECHANICAL EQUIPMENT.
2. ANY EXISTING STREET TREES IMPACTED BY NEW DRIVEWAYS SHALL BE RELOCATED OR REPLACED.
3. PROTECT EXISTING TREES TO REMAIN PER CITY OF BELLEVUE BMP 1101.
4. INSTALL SIGN POSTS PER CITY OF BELLEVUE DETAIL SG-100-1.
5. REFER TO SHEETS C0.01 & C0.02 FOR GENERAL NOTES.

SHEET NOTES:

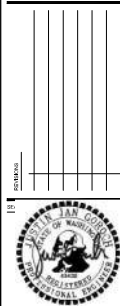
1. CEMENT CONCRETE SIDEWALK PER DETAIL 12-700
2. WHEEL STOP PER DETAIL 12-700
3. "NO PARKING FIRE LANE" SIGN PER DETAIL 12-700
4. FIRE LANE STRIPING PER DETAIL 12-700
5. "STOP" SIGN PER DETAIL 12-700
6. 4" WIDE WHITE PAINT LINE.
7. INSTALL END OF ROAD MARKER SIGN PER DETAIL 12-700
8. BIKE SHELTER, REFER TO LANDSCAPE PLANS.
9. PROPOSED SEGMENTAL BLOCK RETAINING WALL, WALL TO BE BIDDER DESIGNED.
10. PROPOSED DRIVEWAY APPROACH PER CITY OF BELLEVUE DETAIL SW-150-1
11. CEMENT CONCRETE PAVEMENT PER DETAIL 12-700
12. STREET NAME SIGN TYPE 1 PER CITY OF BELLEVUE DETAIL SG-130-1.
13. ABOVE GRADE BOARDWALK STRUCTURE PER DETAIL 12-700
14. REFER TO LANDSCAPE PLANS FOR OPEN SPACE DESIGN.
15. CONNECT TO EXISTING SIDEWALK.
16. PAVERS, REFER TO LANDSCAPE PLANS.
17. CURB RAMP PER DETAIL 12-700
18. 6" TALL FENCE, REFER TO LANDSCAPE PLANS.



TYPICAL ON-SITE ROAD SECTION

SCALE: NTS

BCRA



Date: 2018-12-01 to 2021-12-01

PROJECT
BELLEVUE DOL TOWNHOMES
132ND AVENUE, BELLEVUE, WA 98005

SITE AND
SURFACING PLAN

BCRA
SHEET



C2.01

DESIGN REVIEW

BELLEVUE DOL TOWNHOMES

GENERAL NOTES

- This document has been designed in accordance with the requirements of the International Building Code (IBC) and the International Residential Code (IRC) for the minimum requirements for the construction of the building.
- Intentionally, unless otherwise noted, all dimensions are in feet and inches.
- It is the responsibility of the contractor to verify all dimensions and materials used in the construction of the building.
- It is the responsibility of the contractor to verify all dimensions and materials used in the construction of the building.

DESIGN DATA

- Reference to the design of the building shall be made to the design of the building.
- Reference to the design of the building shall be made to the design of the building.

MATERIALS

- All materials, including but not limited to, shall be of the highest quality and shall be used in accordance with the requirements of the International Building Code (IBC) and the International Residential Code (IRC).

QUALITY ASSURANCE SPECIFICATIONS

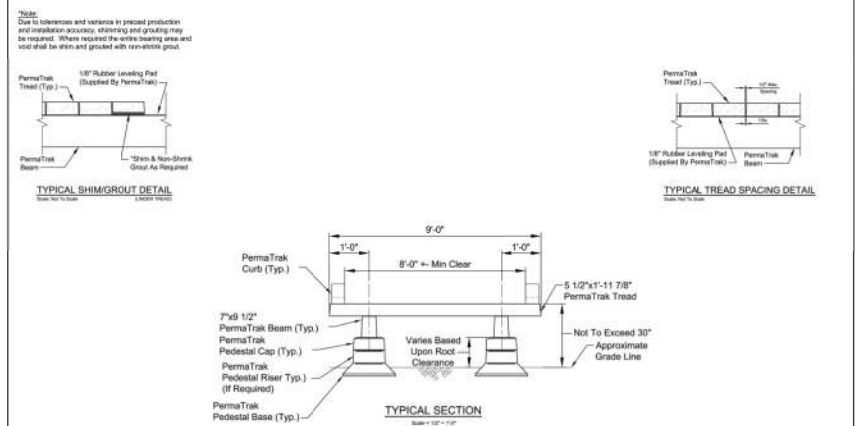
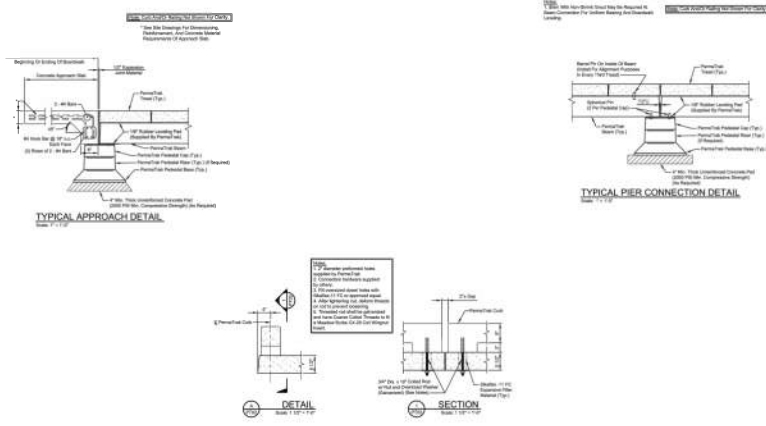
- Materials shall be of the highest quality and shall be used in accordance with the requirements of the International Building Code (IBC) and the International Residential Code (IRC).
- Materials shall be of the highest quality and shall be used in accordance with the requirements of the International Building Code (IBC) and the International Residential Code (IRC).
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- Materials shall be of the highest quality and shall be used in accordance with the requirements of the International Building Code (IBC) and the International Residential Code (IRC).

PROJECT COMPONENTS

COMPONENT	DESCRIPTION
PERMA-TRAK TRAIL	PERMA-TRAK TRAIL
PERMA-TRAK TRAIL	PERMA-TRAK TRAIL
PERMA-TRAK TRAIL	PERMA-TRAK TRAIL
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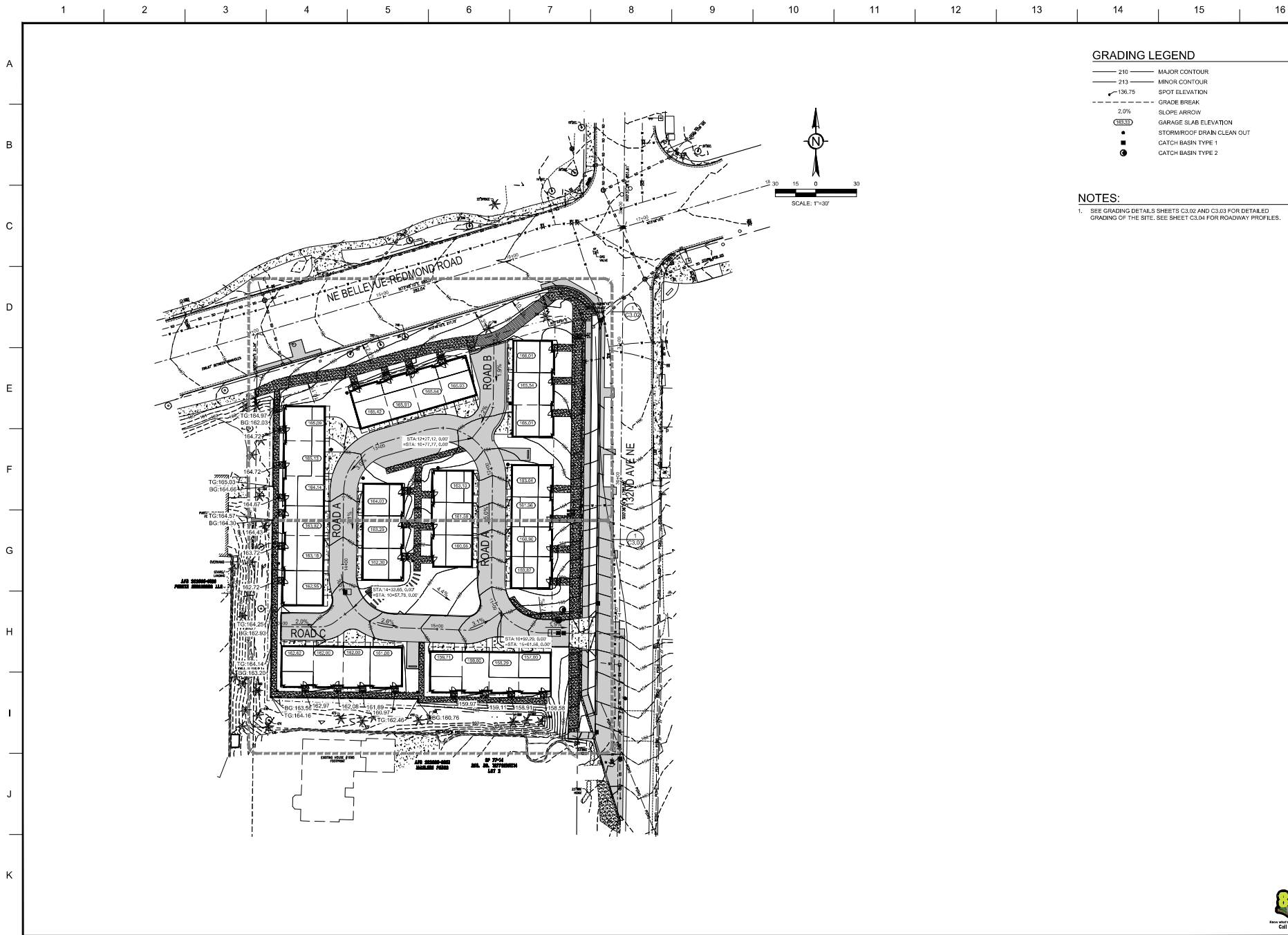


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PERMA-TRAK TRAIL	PERMA-TRAK TRAIL
PERMA-TRAK TRAIL	PERMA-TRAK TRAIL

BOARDWALK DETAILS

SCALE: NTS 1



BCRA



PROJECT
BELLEVUE DOL TOWNHOMES
 BELLEVUE, WA 98005

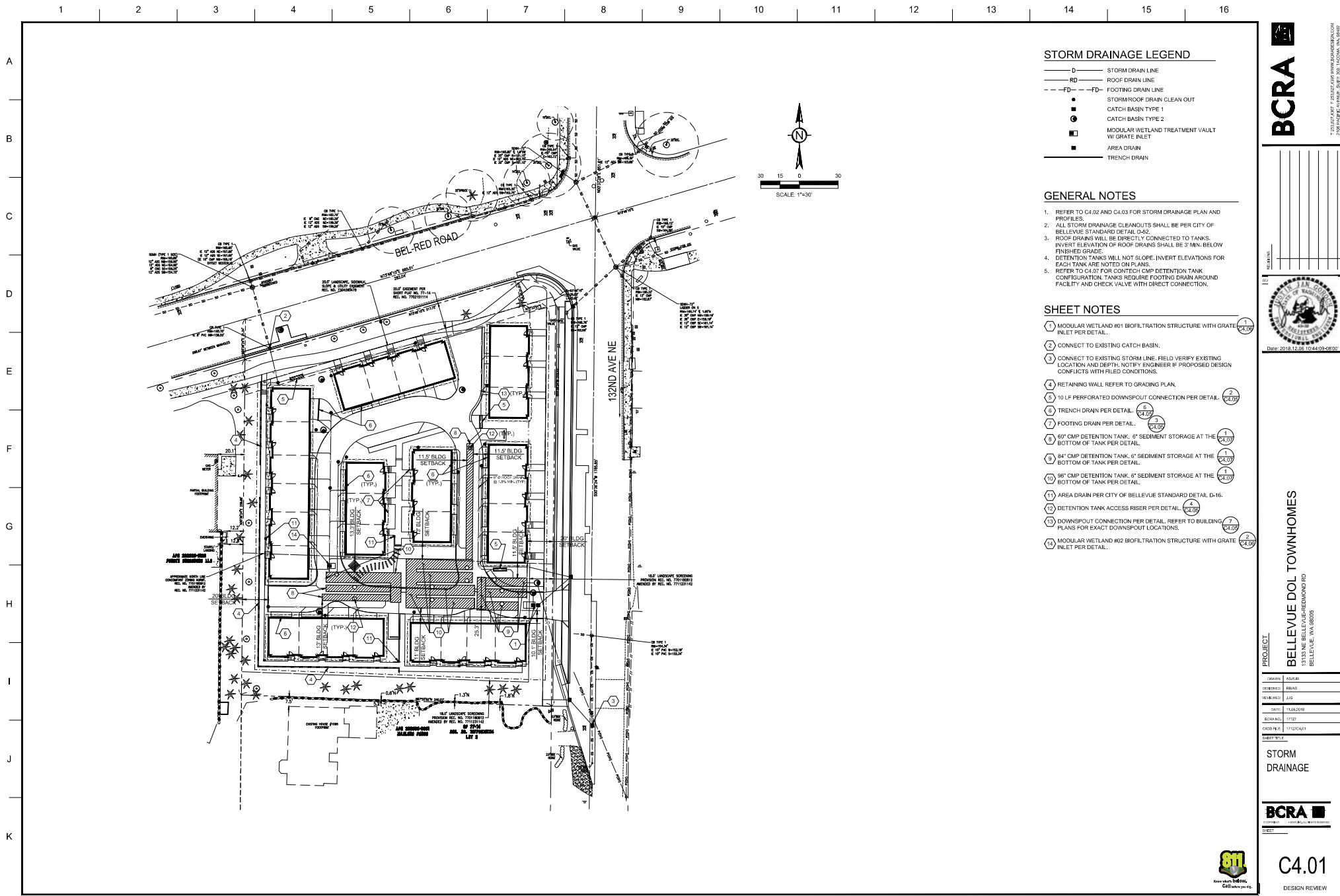
DESIGNER	BCRA
CLIENT	BCRA
DATE	11/8/2018
PROJECT NO.	17127C3.01
DATE	11/8/2018
PROJECT NO.	17127C3.01

GRADING PLAN

BCRA

C3.01

DESIGN REVIEW



STORM DRAINAGE LEGEND

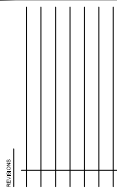
- D — STORM DRAIN LINE
- RD — ROOF DRAIN LINE
- FD — FOOTING DRAIN LINE
- STORM/ROOF DRAIN CLEAN OUT
- CATCH BASIN TYPE 1
- CATCH BASIN TYPE 2
- MODULAR WETLAND TREATMENT VAULT W/ GRATE INLET
- AREA DRAIN
- TRENCH DRAIN

GENERAL NOTES

- REFER TO C4.02 AND C4.03 FOR STORM DRAINAGE PLAN AND PROFILES.
- ALL STORM DRAINAGE CLEANOUTS SHALL BE PER CITY OF BELLEVUE STANDARD DETAIL D-42.
- ROOF DRAINS WILL BE DIRECTLY CONNECTED TO TANKS. INVERT ELEVATION OF ROOF DRAINS SHALL BE 3" MIN. BELOW FINISHED GRADE.
- DETENTION TANKS WILL NOT SLOPE. INVERT ELEVATIONS FOR EACH TANK ARE NOTED ON PLANS.
- REFER TO C4.07 FOR CONTECH CMP DETENTION TANK CONFIGURATION. TANKS REQUIRE FOOTING DRAIN AROUND FACILITY AND CHECK VALVE WITH DIRECT CONNECTION.

SHEET NOTES

- MODULAR WETLAND #01 BIOFILTRATION STRUCTURE WITH GRATE INLET PER DETAIL.
- CONNECT TO EXISTING CATCH BASIN.
- CONNECT TO EXISTING STORM LINE. FIELD VERIFY EXISTING LOCATION AND DEPTH. NOTIFY ENGINEER IF PROPOSED DESIGN CONFLICTS WITH FILED CONDITIONS.
- RETAINING WALL REFER TO GRADING PLAN.
- 10 LF PERFORATED DOWNSPOUT CONNECTION PER DETAIL.
- TRENCH DRAIN PER DETAIL.
- FOOTING DRAIN PER DETAIL.
- 80' CMP DETENTION TANK, 6" SEDIMENT STORAGE AT THE BOTTOM OF TANK PER DETAIL.
- 84' CMP DETENTION TANK, 6" SEDIMENT STORAGE AT THE BOTTOM OF TANK PER DETAIL.
- 86' CMP DETENTION TANK, 6" SEDIMENT STORAGE AT THE BOTTOM OF TANK PER DETAIL.
- AREA DRAIN PER CITY OF BELLEVUE STANDARD DETAIL D-16.
- DETENTION TANK ACCESS RISER PER DETAIL.
- DOWNSPOUT CONNECTION PER DETAIL. REFER TO BUILDING PLANS FOR EXACT DOWNSPOUT LOCATIONS.
- MODULAR WETLAND #02 BIOFILTRATION STRUCTURE WITH GRATE INLET PER DETAIL.



Date: 2018.12.06 10:44:09 CEST

PROJECT
BELLEVUE DOL TOWNHOMES
132ND AVE BELLEVUE, WASHINGTON
BELLEVUE, WA 98005

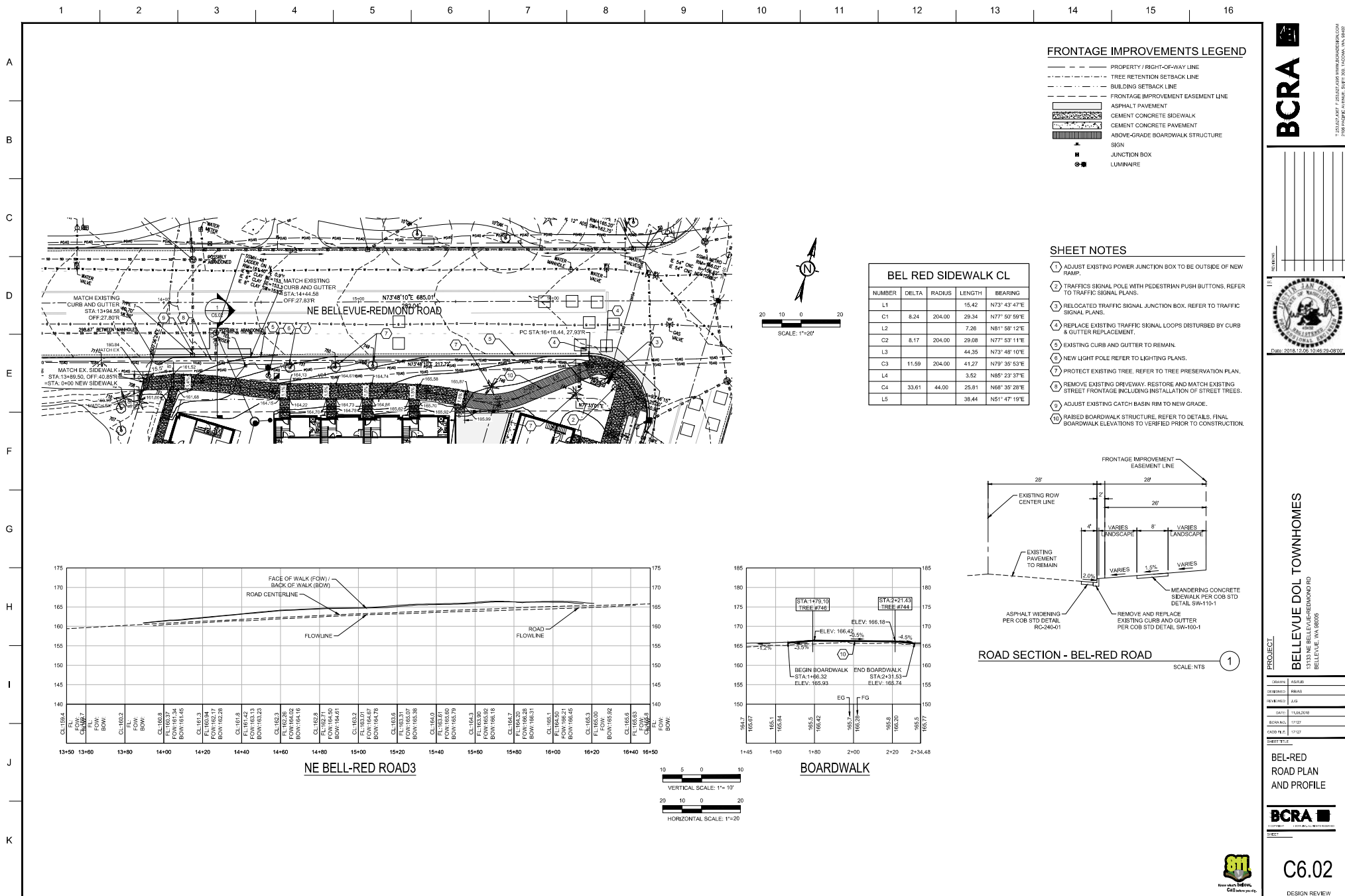
DRAWN	ASR/AB
DESIGNED	ABR/AB
REVIEWED	JUP
DATE	11/05/2018
BY	JUP
CHECKED	JUP
DATE	11/05/2018

STORM DRAINAGE



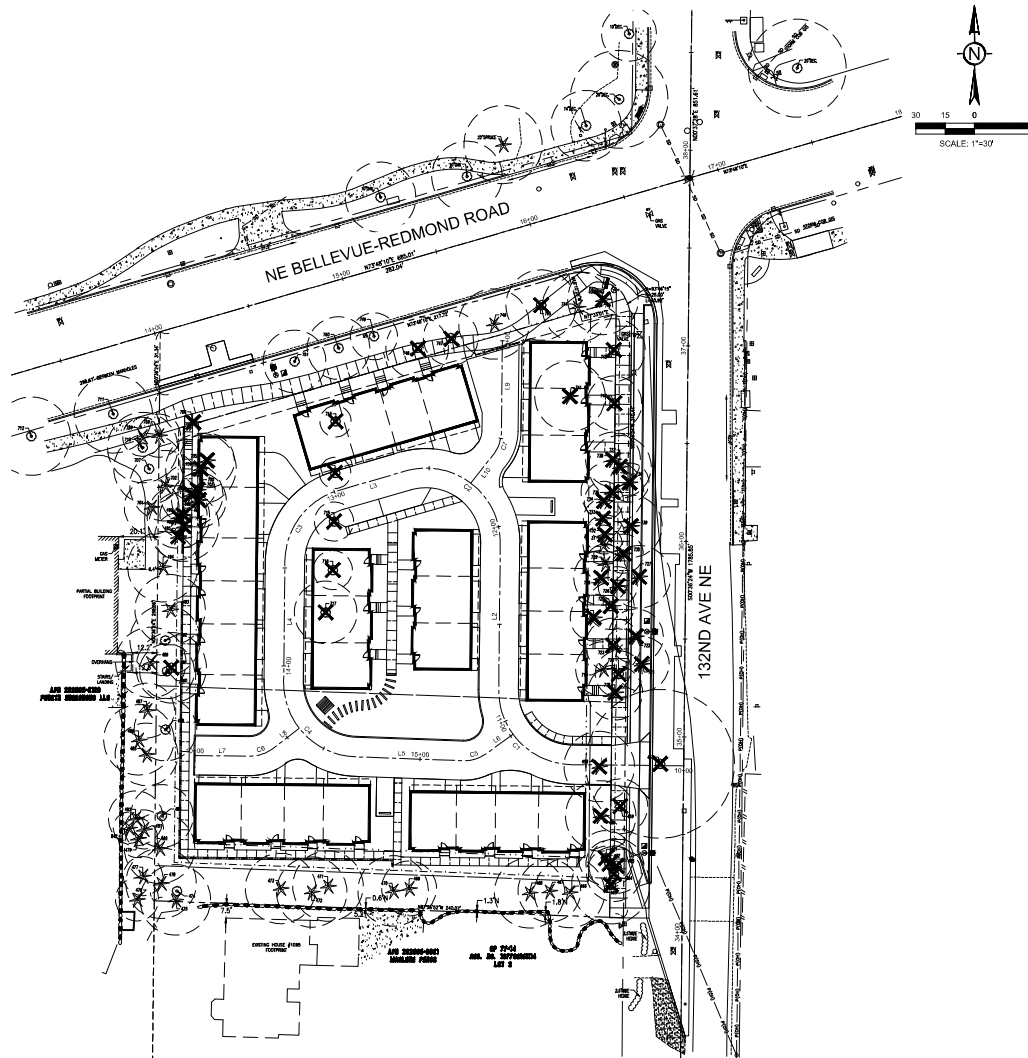
C4.01

DESIGN REVIEW



1 2 3 4 5 6 7 8 9 10 11 12 13 14 15 16

A
B
C
D
E
F
G
H
I
J
K



SITE AND SURFACING LEGEND

- PROPERTY/RIGHT-OF-WAY LINE
- TREE RETENTION SETBACK LINE
- BUILDING SETBACK LINE
- ABOVE-GRADE BOARDWALK STRUCTURE
- RETAINING WALL
- SIGN
- PRECAST CONCRETE WHEELSTOP
- JUNCTION BOX
- LUMINAIRE
- EXISTING VIABLE CONIFEROUS / DECIDUOUS TREE
- TREE DRILINE
- EXISTING TREE TO BE REMOVED

NOTES:

1. REFER TO SHEET C7.02 & C7.03 FOR EXISTING SIGNIFICANT TREE SIZE, SPECIES AND TREE RETENTION CALCULATIONS.



BCRA

17-20247-0077 20247-0097 WWW.BCRAVA.COM
2008 PACIFIC AIRWAY TRUST, SHEET 003, TACOMA, WA 9800



Date: 2018.12.26 15:46:57-52907

PROJECT
BELLEVUE DOL TOWNHOMES
132ND AVE NE, BELLEVUE-REDMOND RD
BELLEVUE, WA 98005

DRAWN	ASAF/AB
DESIGNED	AB/AB
REVIEWED	JAP
DATE	11/05/2018
REVISION	1/1/17
CAD FILE	171202C07A00
SHEET TITLE	

TREE
PRESERVATION
PLAN



C7.01

DESIGN REVIEW

ID	2	3	4	5	6	7	8	9	10	11		12																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																					
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[illegible][illegible]

OFF SITE POTENTIALLY IMPACTED TREES

	1	2	3	4	5	6	7	8	9	10		11				
										CR2/192A/00						
#	Tree Tag #	Species ID	DBH inches	Girth inches	Adj. DBH inches	Drip-line radius feet	Wood form	OK in Grove	Health	Defects/Comments	Retain	Remove	Radius in feet			
											Value	Radio-value	N	W	E	S
1	437	Cottonwood	33	33	33	18			Poor	Previous Ivy @ root crown up to 40', typical of species, vertical crack @ root crown up to 4' towards north, large cavity @ root crown up to 1' towards east. Slight lean towards west, dead wood, broken branches, moss and lichen, @ over crown, epicormic branch formation @ 7'	1	1	18	18	18	18
2	476	Douglas fir	15	15	15	10			OK	Slight lean towards west, asymmetrical canopy towards west, dead wood broken branches, typical of species.	1	1	10	10	10	10
3	477	Douglas fir	13	13	13	12			OK	Ivy @ root crown up to 30', asymmetric canopy towards west, dead wood, broken branches, suppressed canopy, typical of species.	1	1	12	12	12	12
4	478	Douglas fir	8	8	8	12			OK	Asymmetrical canopy towards west, typical of species.	1	1	12	12	12	12
5	481	Douglas fir	10	10	10	14			OK	Asymmetrical canopy towards west, dead wood, broken branches, small crack @ 5' towards west, fine felling sap, horizontal crack @ 2' towards north, typical of species.	1	1	14	14	14	14
6	482	Douglas fir	8	8	8	8		Y	Fair	Suppressed canopy, low live crown ratio < 10%, dead wood, broken branches, typical of species.	1	1	8	8	8	8
7	483	Douglas fir	12	12	12	13		Y	Fair	Self-corrected lean towards east, dead wood, broken branches, horizontal crack @ 20' towards southwest, typical of species.	1	1	13	13	13	13
8	485	Douglas fir	13	13	13	14			OK	Asymmetrical canopy towards west, typical of species.	1	1	14	14	14	14
9	486	Douglas fir	16	16	16	16			OK	Asymmetrical canopy towards west, co-dominant canopy, dead wood, broken branches, typical of species.	1	1	16	16	16	16
10	487	Douglas fir	13	13	13	14			OK	Co-dominant canopy, asymmetrical canopy towards west, ivy @ root crown up to 10', slight lean towards west, typical of species.	1	1	14	14	14	14
11	488	Douglas fir	16	16	16	15			OK	Co-dominant canopy, asymmetrical canopy towards west, dead wood, broken branches, typical of species.	1	1	15	15	15	15
12	701	Decidua cedar	10	10	10	16			OK	Asymmetrical canopy towards west.	1	1	16	16	16	16
13	707	Black locust	9.9	12.5	20	20		Y	Fair	Co-dominant leaders with included bark at @ root crown, lean towards west < 45°, typical of species.	1	1	20	20	20	20
14	709	Douglas fir	9	9	9	13			OK	Suppressed canopy, asymmetrical canopy towards north, dead wood, broken branches, typical of species.	1	1	13	13	13	13
15	710	Black locust	9.9	12.5	18	18			OK	Co-dominant leaders with included bark at @ root crown, ivy @ root crown up to 10', typical of species.	1	1	18	18	18	18
16	711	Oak	14	14	14	20			OK	Right of way trees, asymmetrical canopy towards north, previous failure at 10' towards south, typical of species.	1	1	20	20	20	20
17	712	Oak	13	13	13	20			OK	Right of way trees, co-dominant leaders with included bark at @ 13', structurally poor.	1	1	20	20	20	20
18	721	Douglas fir	11	11	11	10			OK	Self-corrected lean towards west, typical of species, calloused around @ 1' towards east.	1	1	10	10	10	10
19	722	Douglas fir	11	11	11	14			OK	Self-corrected lean towards west, co-dominant canopy, typical of species.	1	1	14	14	14	14
20	727	Douglas fir	16	16	16	16			Poor	Tape volume, previous top loss @ 30', abnormal bark, shedding bark, carpenter ants, dead wood, broken branches.	1	1	16	16	16	16
21	736	Douglas fir	12	12	12	14			OK	Asymmetrical canopy towards east, co-dominant canopy, dead wood, broken branches, dead twigs, typical of species.	1	1	14	14	14	14
22	739	Douglas fir	13	13	13	10			OK	Previous top loss, typical of species.	1	1	10	10	10	10

TREE DENSITY CALCULATION (TREE #)	
TOTAL NUMBER OF ONSITE TREES 8" OR GREATER DEEMED "SIGNIFICANT"	73
TOTAL NUMBER OF SIGNIFICANT PERIMETER TREES (HEALTHY AND UNHEALTHY)	38
TOTAL NUMBER OF NUMBER OF INTERIOR TREES (HEALTHY AND UNHEALTHY)	35
TOTAL NUMBER OF SIGNIFICANT PERIMETER TREES (HEALTHY AND UNHEALTHY)	38
SAFETY HAZARD TREES TO BE REMOVED	11
TREES REMOVED FOR REQUIRED FRONTAGE IMPROVEMENTS	0
NET TOTAL PERIMETER TREES	24
TOTAL DIAMETER OF INTERIOR TREES (INCHES)	474
TOTAL DIAMETER OF TREES LISTED AS POOR (INCHES)	105
NET TOTAL OF DIAMETER (INCHES)	369
REQUIRED RETENTION 15% OF INTERIOR TREES (474*.15)	71
ACTUAL % OF RETAINED INTERIOR TREES (103 /474)	22%



BCRA

11/5/2018 3:49:28 PM \\JUPITER\PROJECTS\17000\17127\001 - BELLEVUE DOL TOWNHOMES\DWG\CD SET\17127\01-02 By: RBALTAZAR



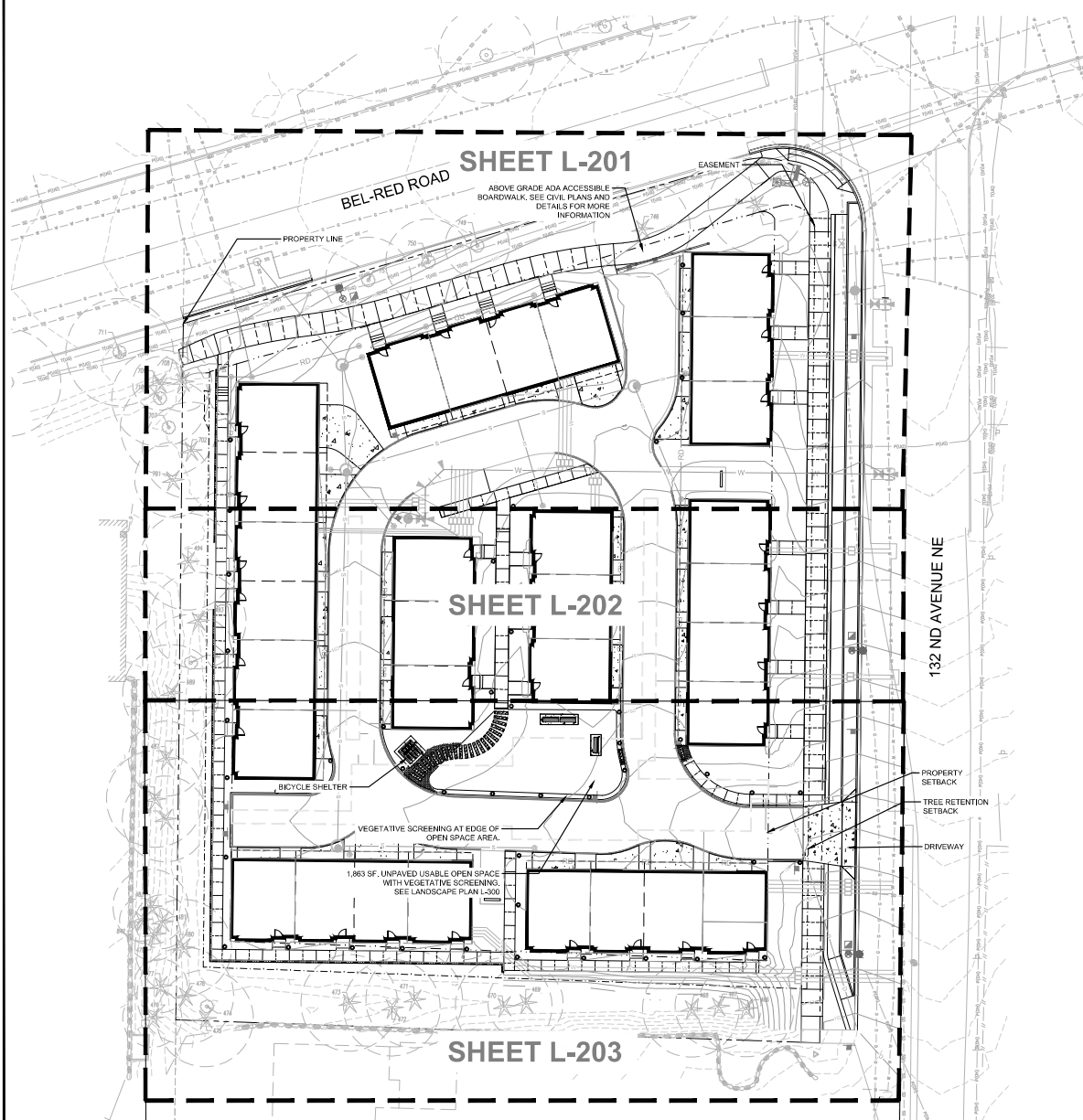
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PROJECT
BELLEVUE DOL TOWNHOMES
13715 NE BELLEVUE/REGIMOND RD
BELLEVUE, WA 98006

DRAWN	ASR/AB
DESIGNED	RBAL
ENGINEER	JUP
DATE	11/05/2018
REVISION	17127
CD SET	17127\CD\01-02
SHEET NO.	

TREE
RETENTION
PLANC7.03
DESIGN REVIEW

Date Plotted: Nov. 05, 2018 1:53pm Filename: 17127.00.01 L200 MATERIALS PLAN.dwg Bg: A201



MULTI-FAMILY PLAY AREA CALCULATIONS

PER LUC 20.20.540:

NEW MULTIFAMILY DEVELOPMENTS OF 10 UNITS OR MORE SHALL BE REQUIRED TO PROVIDE A MINIMUM OF UNPAVED, USABLE OPEN SPACE WITH LAWN OR OTHER SOFT SURFACE FOR AN OUTDOOR CHILDREN'S PLAY AREA, PLUS AN ADDITIONAL 50 SQUARE FEET OF USABLE OPEN SPACE FOR EACH ADDITIONAL UNIT BEYOND THE INITIAL 10 UNITS, UP TO A MAXIMUM OF 10,000 SQUARE FEET.

THE MINIMUM DIMENSION SHALL BE 25' BERMS, VEGETATIVE SCREENING OR FENCING SHOULD SEPARATE THE PLAY AREA FROM DRIVING AND PARKING AREAS. RESIDENTS SHOULD HAVE CONVENIENT ACCESS. THE DESIGN SHOULD INVITE A VARIETY OF ACTIVE AND PASSIVE RECREATIONAL ACTIVITIES APPROPRIATE FOR CHILDREN BY UTILIZING UNIQUE NATURAL FEATURES, CREATING GENTLE SLOPES OR BERMS, OR PROVIDING OTHER AMENITIES SUCH AS BENCHES OR PLAY EQUIPMENT.

BELLEVUE DOL TOWNHOMES = 31 UNITS.

EQUATION FOR PLAY AREA REQUIRED:
800 SF BASE + (50 SF x 21 ADDITIONAL UNITS) = 1850 SF PLAY AREA REQUIRED.

EQUATION FOR TOTAL PLAY AREA PROVIDED:
1863 SF PLAY AREA PROVIDED.

LAYOUT AND MATERIAL PLAN SCHEDULE AND NOTES

SCALE: 1" = 20'



PROJECT
BELLEVUE DOL TOWNHOMES
JULIE BELLINGHAM
BELLINGHAM, WASHINGTON 98208

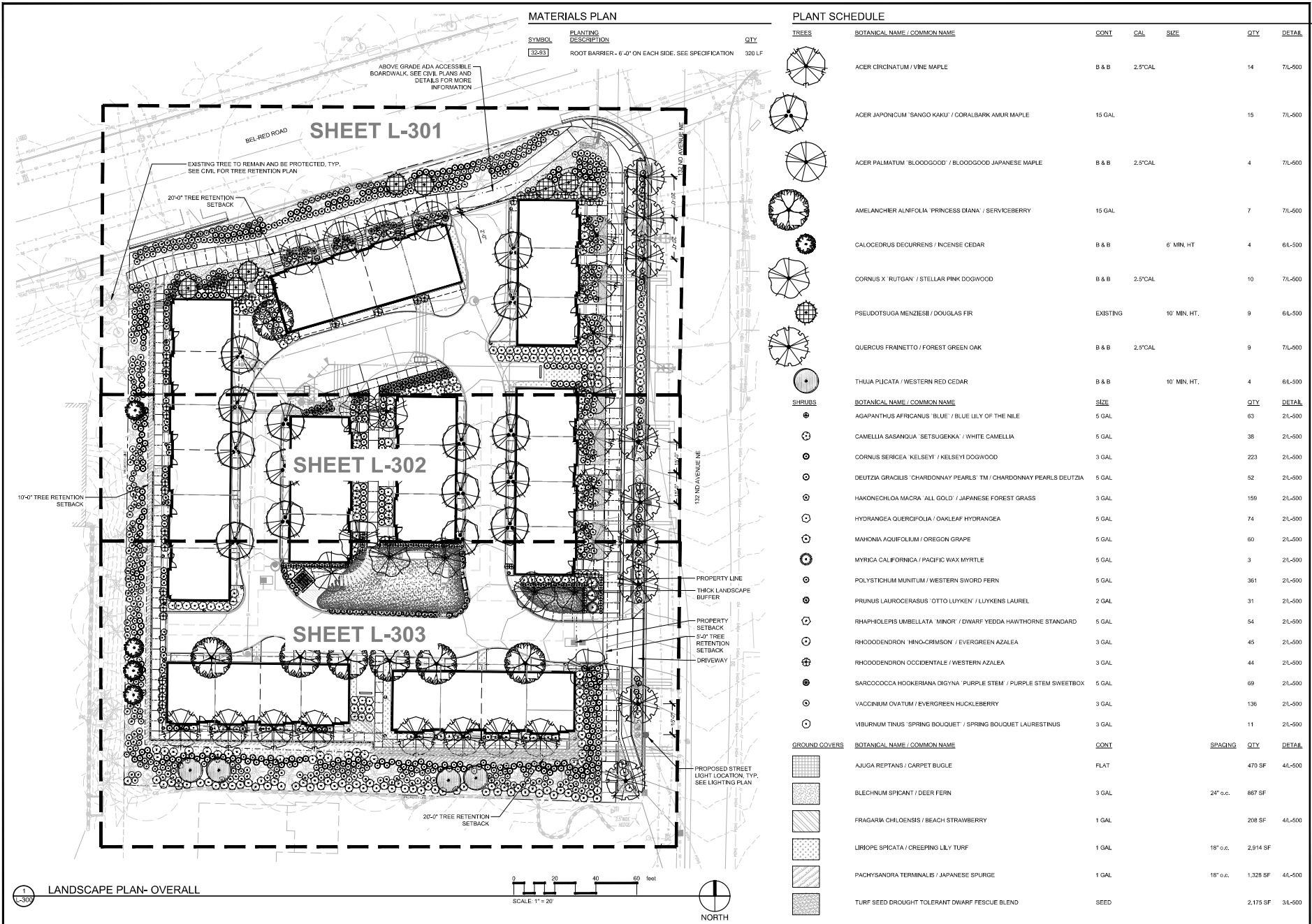
DATE
11.05.18
BCRA NO.
17127
SHEET NO.
SHEET TITLE

OVERALL LAYOUT AND MATERIALS PLAN, AND NOTES



L-200

DESIGN REVIEW



BCRA

STATE OF WASHINGTON
REGISTERED
LANDSCAPE ARCHITECT

JON MCNAMARA
LICENSE NO. 803
EXPIRES ON 06/29/2018

PROJECT

BELLEVUE DOL TOWNHOMES

1105 18th Ave N
Bellevue, WA 98008

DATE

11.05.18

BY

JLOU

CHECKED BY

17/27

DATE

11.05.18

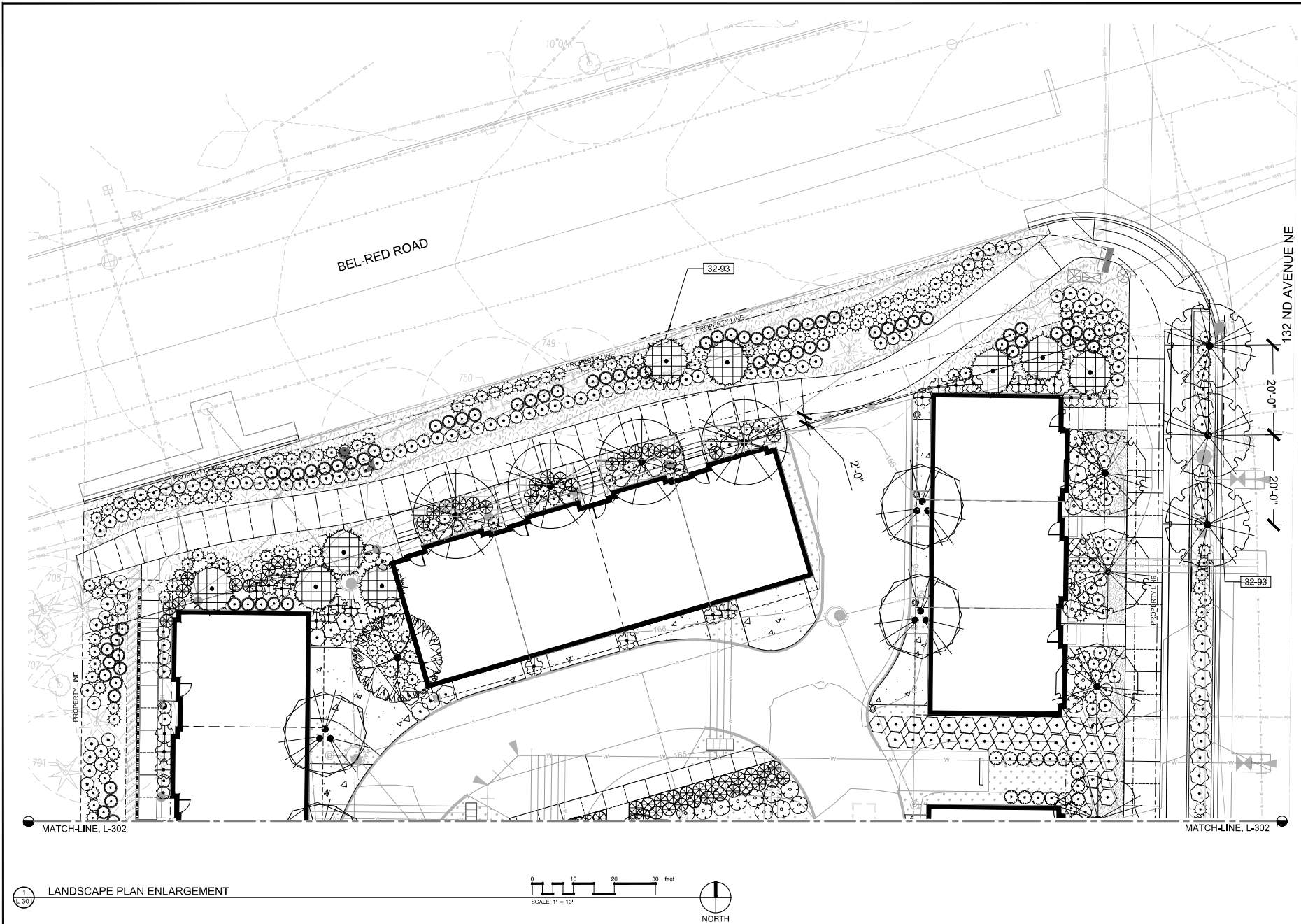
OVERALL LANDSCAPE PLAN AND SCHEDULE

BCRA

L-300

DESIGN REVIEW

Date Plotted: Nov. 05, 2018 - 1:53pm Filename: 17127.00.01 L301 PLANNING PLAN.dwg By: JLOU



BCRA

STATE OF WASHINGTON
REGISTERED
LANDSCAPE ARCHITECT
JON MCNAMARA
LICENSE NO. 803
EXPIRES ON 06/29/2018

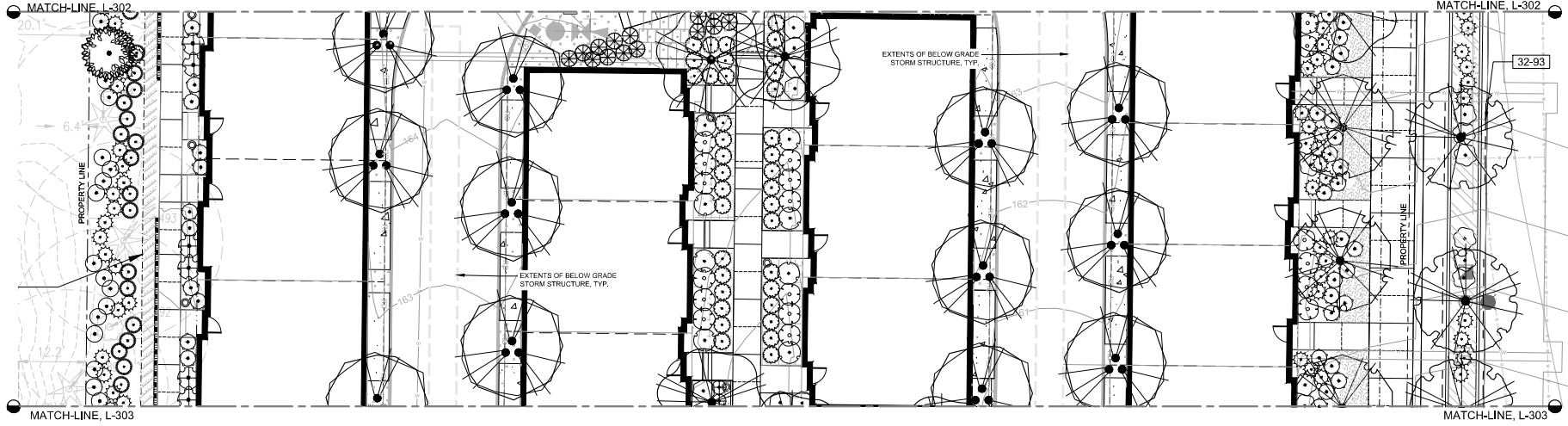
PROJECT
BELLEVUE DOL TOWNHOMES
JULIAN MCNAMARA
BELLEVUE, WASHINGTON 98008

DATE	DESCRIPTION
11.05.18	
11.12.17	
11.12.17	

SHEET TITLE
LANDSCAPE PLAN
ENLARGEMENT

BCRA

SHEET
L-301
DESIGN REVIEW



1
L-302

LANDSCAPE PLAN ENLARGEMENT

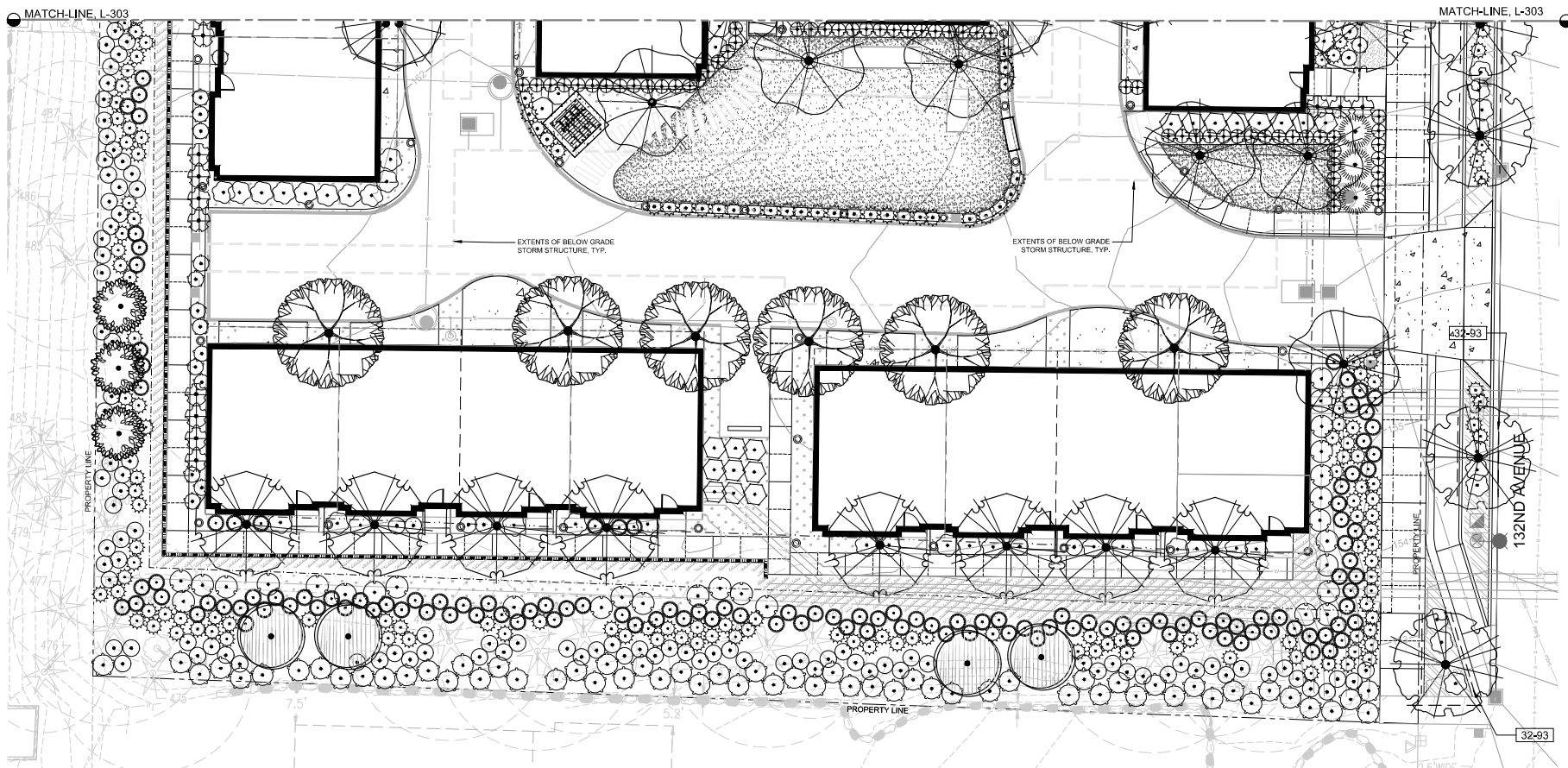
0 10 20 30 feet
SCALE: 1" = 10'



DATE
11.05.18
BCRA NO.
17127
DRW FILE
SHEET FILE

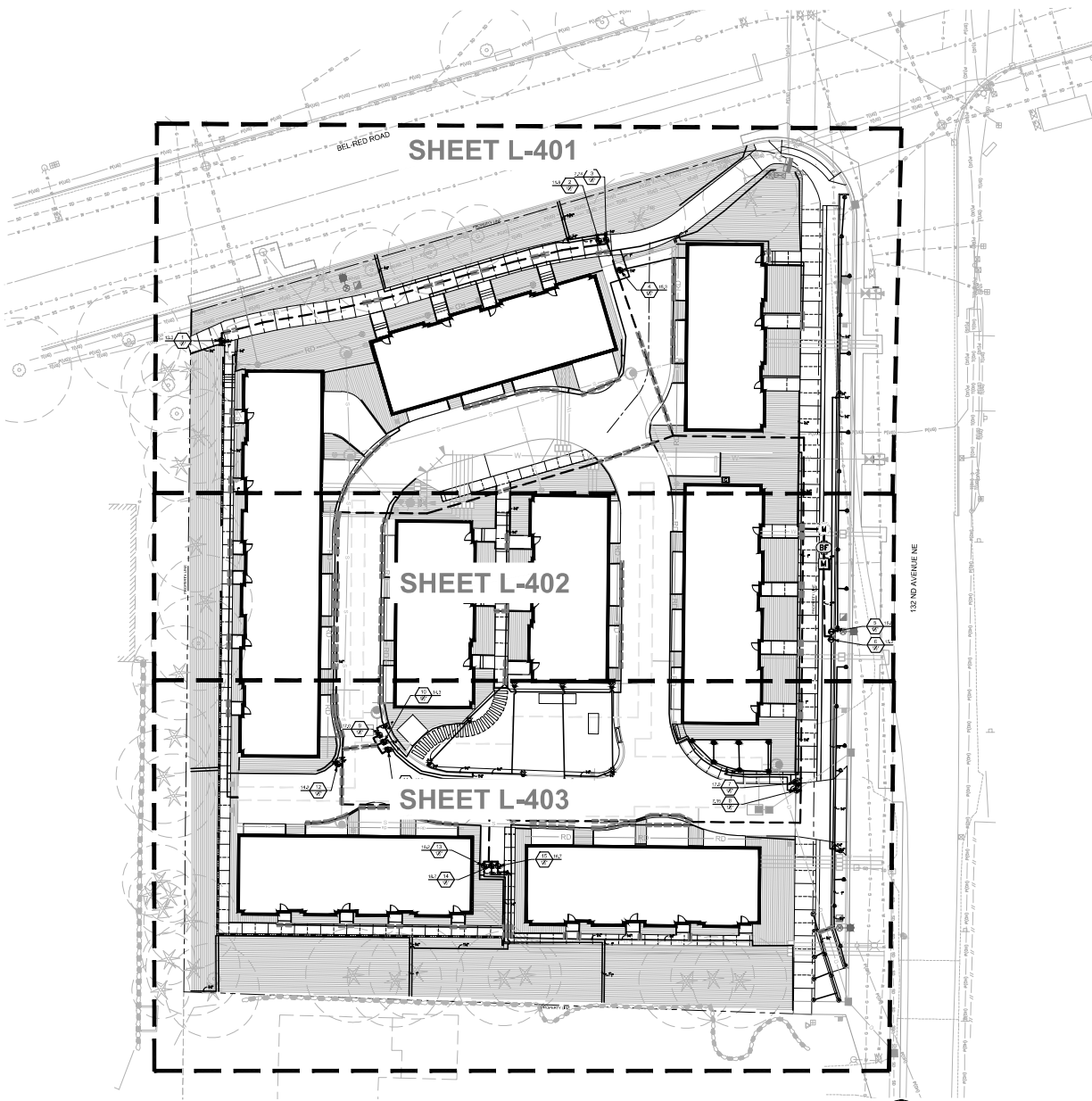
LANDSCAPE PLAN
ENLARGEMENT

Date Plotted: Nov. 05, 2018 - 1:53pm Filename: 17127.00.01 L300 PLANNING PLAN.dwg By: JLOU



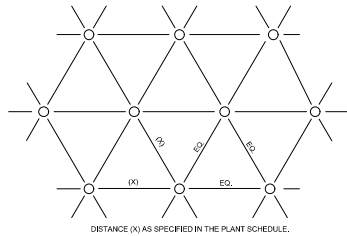
1 IRRIGATION PLAN - OVERALL

0 20 40 60 Feet
SCALE: 1" = 20'
NORTH

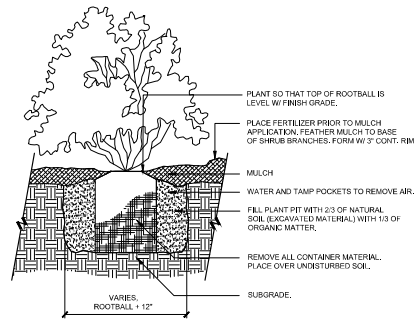


IRRIGATION SCHEDULE

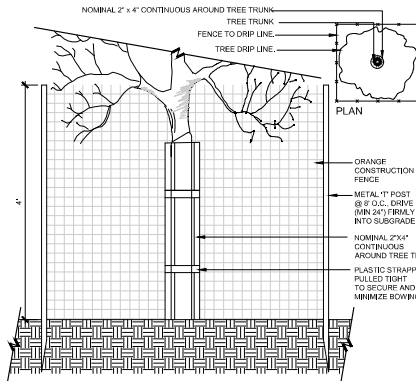
SYMBOL	MANUFACTURER/MODEL/DESCRIPTION	QTY	PSI	DETAIL
	RAIN BIRD 1804-PRS 12 SERIES MPR TURF SPRAY 4.0" POP-UP SPRINKLER WITH CO-MOLDED WIPER SEAL, 1/2" NPT FEMALE THREADED INLET, PRESSURE REGULATING.	7	30	1A-501
	HUNTER PROS-06 5 STRIP SPRAY SHRUB SPRAY, 6.0" POP-UP, CO-MOLDED WIPER SEAL WITH UV RESISTANT MATERIAL.	20	30	1A-501
	HUNTER PROS-06 ADJ SHRUB SPRAY, 6.0" POP-UP, CO-MOLDED WIPER SEAL WITH UV RESISTANT MATERIAL.	11	30	1A-501
	RAIN BIRD 3504-PC TURF ROTOR, 4.0" POP-UP, ADJUSTABLE AND FULL ORBIT.	7	35	3.58 33' 4L-501
	AREA TO RECEIVE DRIPLINE RAIN BIRD XF0CV-08-18 XF0CV ON-SURFACE LANDSCAPE DRIPLINE WITH A HEAVY-DUTY 3/8 PSI CHECK VALVE, 0.9 GPH EMITTERS AT 18" O.C. DRIPLINE LATERALS SPACED AT 18" APART, WITH EMITTERS OFFSET FOR TRIANGULAR PATTERN, GREAT FOR ELEVATION CHANGE. SPECIFY XF INSERT FITTINGS.	14,276 L.F.		4L-502
	RAIN BIRD PEBL-PRS-D 1" 1-1/2", 2" PLASTIC INDUSTRIAL VALVES, LOW FLOW OPERATING CAPABILITY, GLOBE CONFIGURATION, WITH PRESSURE REGULATOR MODULE.	13		2L-502
	WEATHERMATIC 8200CR-XPR BRASS REMOTE CONTROL VALVE WITH PRESSURE REGULATOR	2		2L-502
	RAIN BIRD ESPSLXW6 WITH (2) ESPL0MSM12 32 STATION COMMERCIAL CONTROLLER, MOUNTED ON A PLASTIC WALL MOUNT.	1		6L-501
	WATER METER 1-1/2" EXISTING 1.5" WATER METER	1		
	IRRIGATION LATERAL LINE: CPVC SCHEDULE 40	4,559 L.F.		
	IRRIGATION MAINLINE: CPVC SCHEDULE 40	974.4 L.F.		
	PIPE SLEEVE: CPVC SCHEDULE 40 SLEEVE SHALL BE TWO TIMES THE SIZE OF THE PIPE GOING THROUGH IT.	785.0 L.F.		8L-501
	Valve Control Valve Number Valve Flow Valve Size			



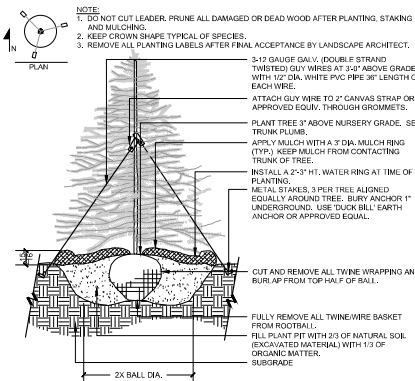
1 PLANT SPACING
1" = 1'



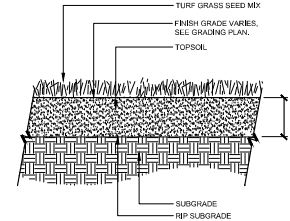
2 SHRUB PLANTING - CONTAINER
1" = 1'-0"



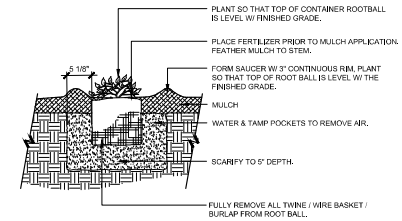
5 EXISTING TREE PROTECTION
1" = 1'-0"



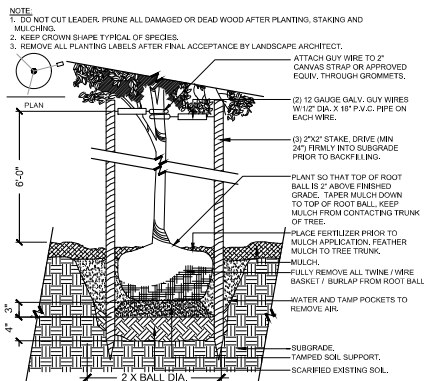
6 EVERGREEN TREE PLANTING
1" = 1'-0"



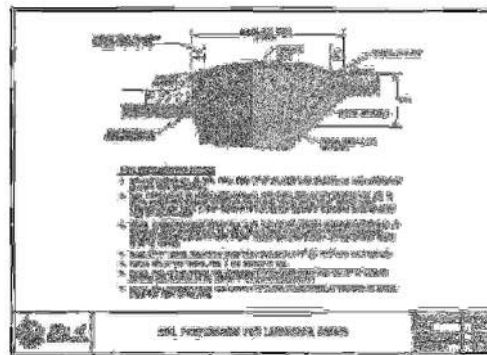
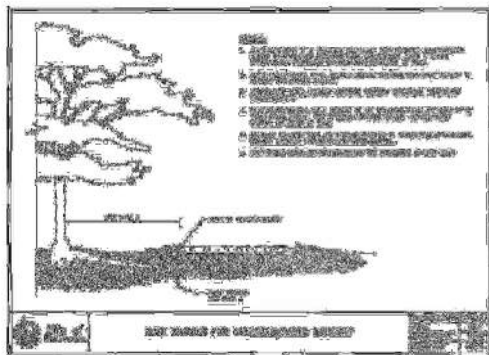
3 TURF
1" = 1'-0"

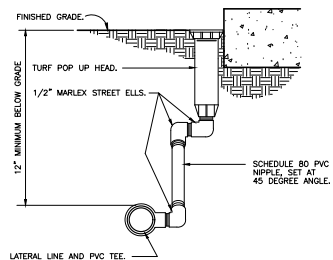


4 GROUNDCOVER PLANTING
1" = 1'-0"

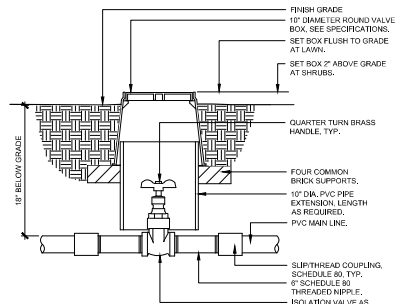


7 DECIDUOUS TREE PLANTING
1" = 1'-0"

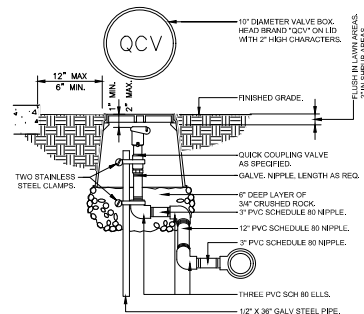




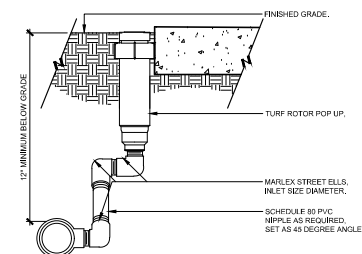
1 TURF / SHRUB SPRAY/ ROTOR MARLEX ASSEMBLY
3" = 1'-0" 328403.13-01



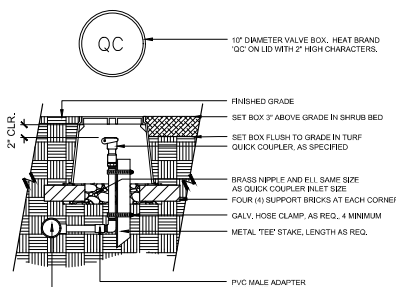
2 BRASS ISOLATION VALVE
1 1/2" = 1'-0" 328406.33-01



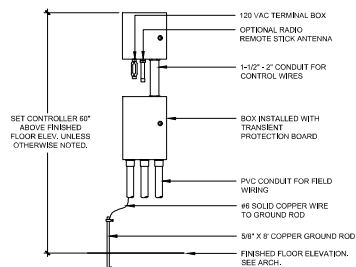
3 QUICK COUPLING VALVE IN BOX
1 1/2" = 1'-0" 328406.43-02



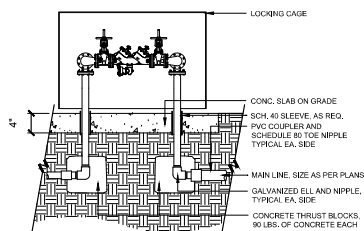
4 TURF ROTOR MARLEX ASSEMBLY
2" = 1'-0" 328403.16-01



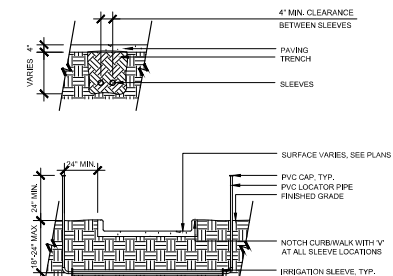
5 QUICK COUPLING VALVE IN BOX
1" = 1'-0" 328406.43-05



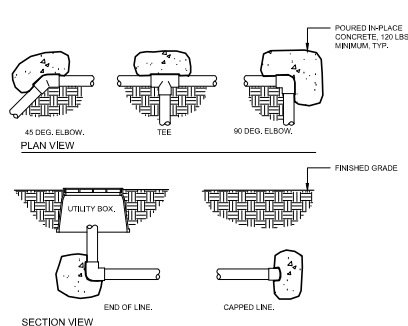
6 WALL MOUNT CONTROLLER
1" = 1'-0" 328406.13-12



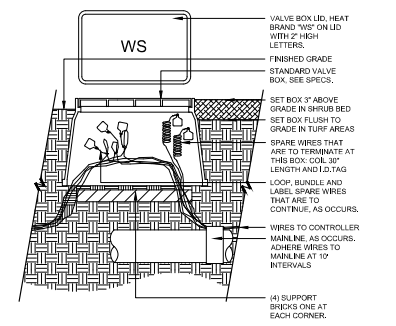
7 DOUBLE CHECK VALVE BACKFLOW DEVICE
1" = 1'-0" 328406.46-05



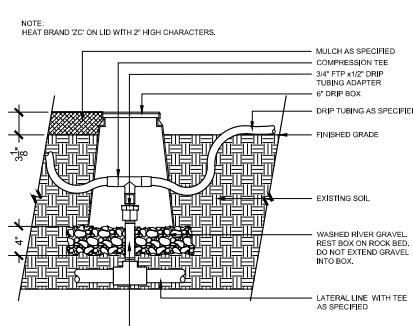
8 IRRIGATION: SLEEVING
3/8" = 1'-0" 328406.76-07



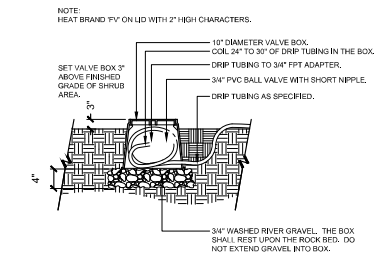
9 THRUST BLOCKING
3/4" = 1'-0" 328406.76-01



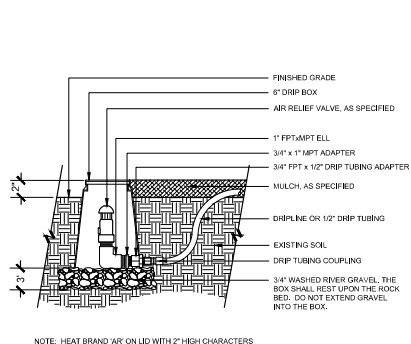
10 WIRE BUNDLE JUNCTION BOX
1" = 1'-0" 328406.79-01



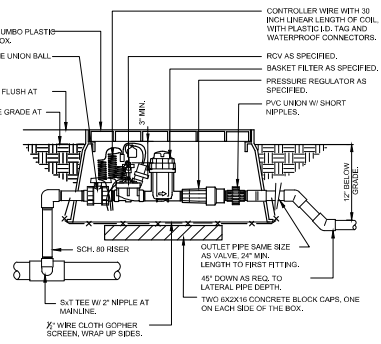
11 DRIPLINE ZONE CONTROL
1 1/2" = 1'-0" 328413.46-03



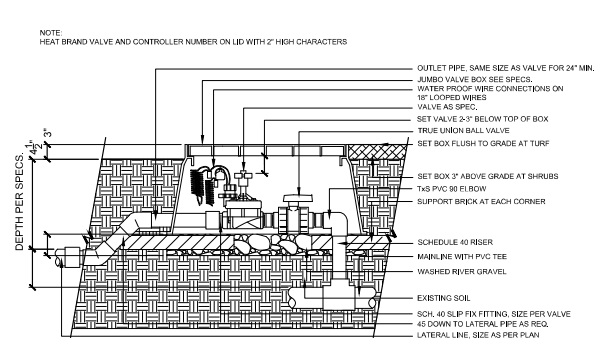
12 DRIP FLUSH VALVE
1" = 1'-0" 328413.46-03



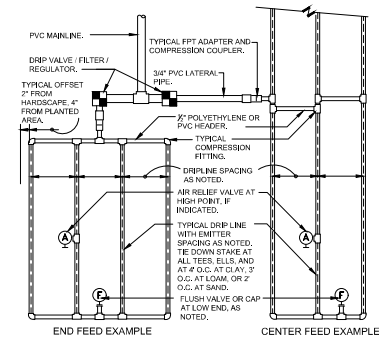
1 1 1/2\"/>



2 1 1/2\"/>



3 1\"/>



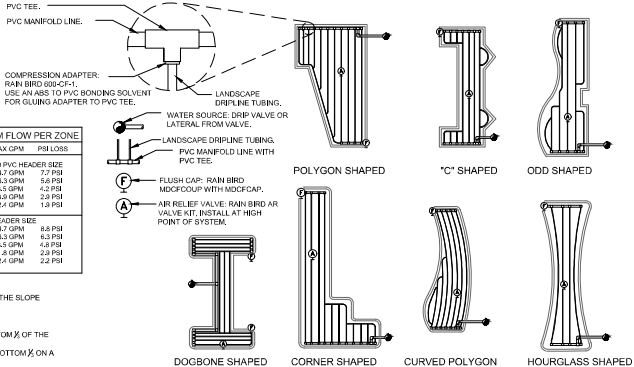
4 3\"/>

MAXIMUM LATERAL LENGTH (FEET)				
PSI	EMITTER FLOW RATE (GPH)			
	12" SPACING		16" SPACING	
	0.6	0.9	0.6 0.9	
10	125	96	175 135	218 171
20	249	191	350 171	442 340
30	307	236	434 333	550 422
40	350	268	495 380	627 171
50	125	96	175 135	218 171
60	125	96	175 135	218 171

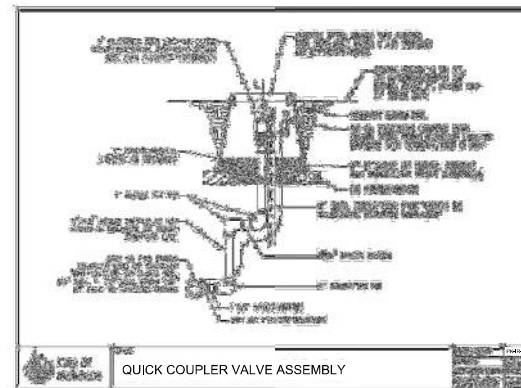
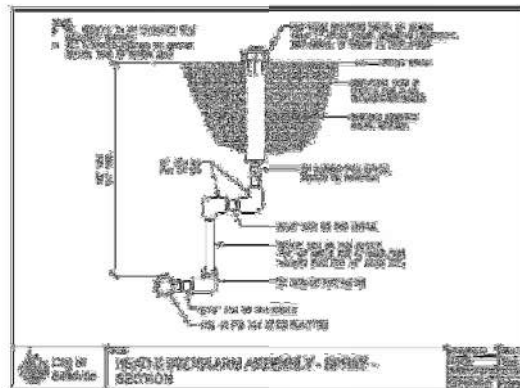
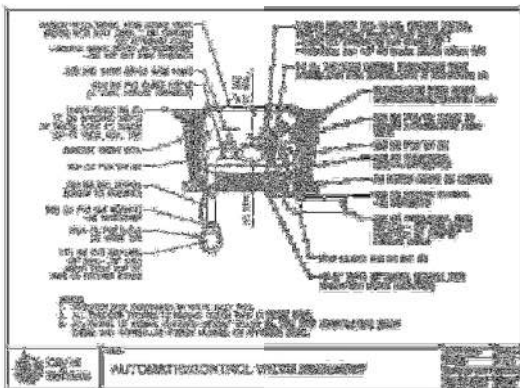
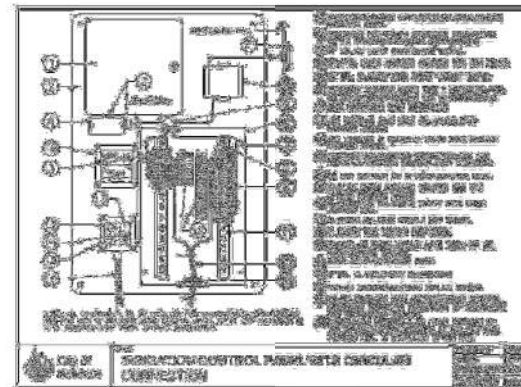
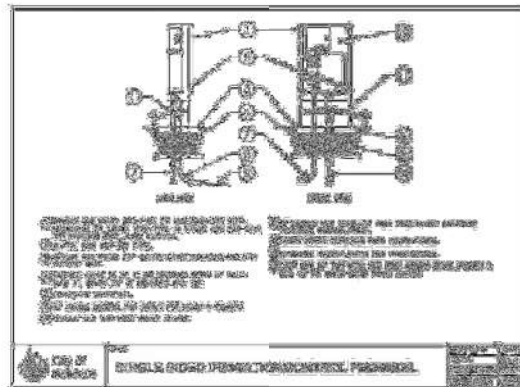
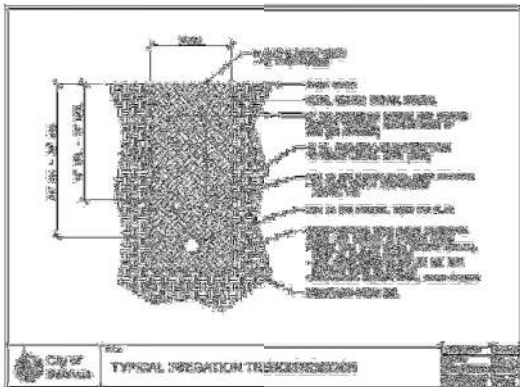
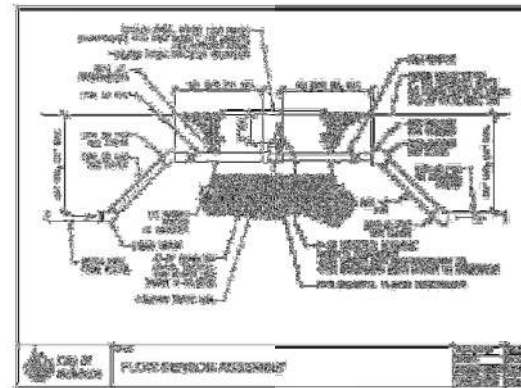
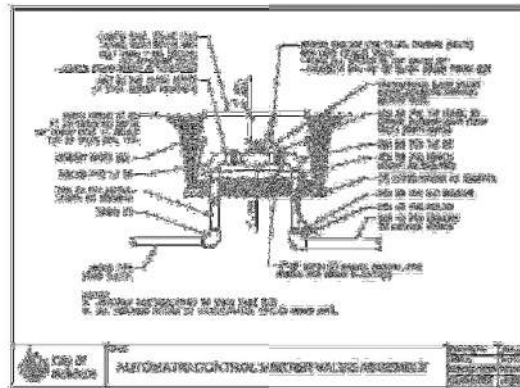
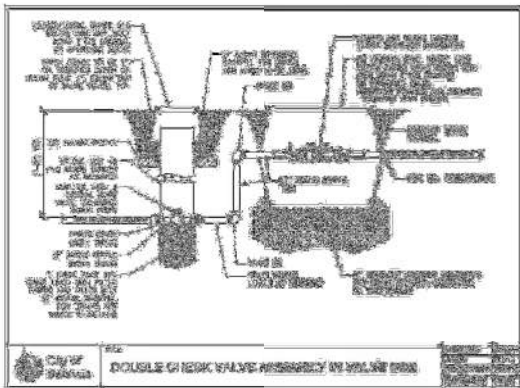
GRID PRECIPITATION RATES (IN/HR)			
EMITTER SPACING	LATERAL SPACING	EMITTER FLOW RATE	
		0.5	0.9
12	12	0.86	1.44
18	18	0.89	1.03
24	24	0.75	0.41

LATERAL FLOW PER 100 FT (GPM)			
EMITTER FLOW	12" SPACING	18" SPACING	24" SPACING
0.6 GPH	1.0 GPM	0.67 GPM	0.50 GPM
0.9 GPH	1.5 GPM	1.0 GPM	0.75 GPM

- SLOPED CONDITION NOTE:
1. DRIPLINE LATERALS SHOULD FOLLOW THE CONTOURS OF THE SLOPE WHENEVER POSSIBLE.
 2. INSTALL AIR RELIEF VALVE AT HIGHEST POINT.
 3. NORMAL SPACING WITHIN THE TOP 1/2 OF SLOPE.
 4. INSTALL DRIPLINE AT 25% GREATER SPACING AT THE BOTTOM 1/2 OF THE SLOPE.
 5. WHEN ELEVATION CHANGE IS 10 FT OR MORE, ZONE THE BOTTOM 1/2 ON A SEPARATE VALVE.



328413.55-07



BELLEVUE DOL TOWNHOMES

DESIGN REVIEW SUBMITTAL

BELLEVUE, WA

SEPTEMBER 17, 2018



Introduction

Bellevue DOL Townhomes is a community of 31 homes located in the revitalizing Bel-Red District. This project will provide a distinguished street scene with front doors, living spaces, and public trails along both 132nd Ave NE and Bel Red Road, serving as a catalyst as this District transitions into a dynamic destination within the City.

Existing Conditions

This 1.64-acre site is currently occupied by the Bellevue Driver Licensing Office. A two-story office building is adjacent to the site on the west in the BR-ORT district. Various commercial uses are located to the north of the site across Bel-Red Road in the BR-CR district, and a two-story office is to the east on the other side of 132nd Ave NE in BR-ORT. The south side of the project is abutted by a single family home within the R-3.5 zone. No known critical areas exist within the site.

Site Plan

The project falls within the Bel-Red Office, Residential, Transition Design District (BR-ORT) which allows multifamily residential as a permitted use. Eight townhome buildings with a total of 31 homes are planned within the 20 foot front yard setbacks along Bel-Red Road and 132nd Ave NE, the 20 foot side yard setback along the east boundary, and the 30 foot rear yard setback along the south. Each townhome building is separated by at least 20 feet, to honor the transition edge development standard as this site borders a residential zone.

A total of 49,034 square feet of gross floor area minus garages is planned for the community. This total is within the 53,632 square feet permitted for this site at the 0.75 maximum floor area ratio. This project also meets the maximum impervious surface and lot coverage requirements as shown in this package. Each home has two parking spaces within a garage, which is the maximum allowed per the BR-ORT District. Guest spaces are not required in this zone, however four spaces have been provided on the site.

Architecture

Contemporary architectural forms and materials have been designed for this site to provide a stately presence along the project frontage. The proposed materials, finishes, and color details are specified within this document. The building facades incorporate techniques such as setbacks, offsets, and material changes to break up the massing of the structures and create an interesting, varying elevation. The building height proposed for the project is about 37 feet, which is below the 45 foot maximum allowed.

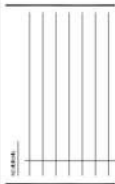
Landscape

A hierarchical layering of shrubs and trees has been designed for the project to soften building edges, provide buffering and screening, and define walkways and entries. Multifamily play areas are designed within the site to provide open space and gathering areas for residents. This project provides 2,291 square feet of play area which is above the 1,850 square foot requirement for 31 homes as shown in the landscape section of this package. Four bicycle parking spaces are provided as part of the landscape plan to meet the requirement of 3.1 spaces.

The perimeter of the site contains many existing trees, the majority of which have been preserved as part of this site plan, and will help maintain the existing landscape buffer surrounding the project. A total of 53% of the existing 73 trees are to be retained, which exceeds the 30% requirement.



10900 NE 8th Street #1120
Bellevue, WA 98004
425.453.5368



DATE: 9/17/18

BY: [Signature]

PROJECT: BELLEVUE DOL TOWNHOMES

LOCATION: BELLEVUE, WA

SCALE: 1/8" = 1'-0"

DATE: 9/17/18

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SCALE: 1/8" = 1'-0"

DATE: 9/17/18

BY: [Signature]

PROJECT: BELLEVUE DOL TOWNHOMES

LOCATION: BELLEVUE, WA

SCALE: 1/8" = 1'-0"

DATE: 9/17/18

BY: [Signature]

PROJECT: BELLEVUE DOL TOWNHOMES

LOCATION: BELLEVUE, WA

SCALE: 1/8" = 1'-0"

DATE: 9/17/18

BY: [Signature]

PROJECT: BELLEVUE DOL TOWNHOMES

LOCATION: BELLEVUE, WA

SCALE: 1/8" = 1'-0"

DATE: 9/17/18

BY: [Signature]

PROJECT: BELLEVUE DOL TOWNHOMES

LOCATION: BELLEVUE, WA

SCALE: 1/8" = 1'-0"

DATE: 9/17/18

BY: [Signature]

PROJECT: BELLEVUE DOL TOWNHOMES

LOCATION: BELLEVUE, WA

SCALE: 1/8" = 1'-0"

DATE: 9/17/18

BY: [Signature]

PROJECT: BELLEVUE DOL TOWNHOMES

LOCATION: BELLEVUE, WA

SCALE: 1/8" = 1'-0"

DATE: 9/17/18

BY: [Signature]

PROJECT: BELLEVUE DOL TOWNHOMES

LOCATION: BELLEVUE, WA

SCALE: 1/8" = 1'-0"

DATE: 9/17/18

BY: [Signature]

PROJECT: BELLEVUE DOL TOWNHOMES

LOCATION: BELLEVUE, WA

SCALE: 1/8" = 1'-0"

DATE: 9/17/18

BY: [Signature]

PROJECT: BELLEVUE DOL TOWNHOMES

LOCATION: BELLEVUE, WA

SCALE: 1/8" = 1'-0"

DATE: 9/17/18

BY: [Signature]

PROJECT: BELLEVUE DOL TOWNHOMES

LOCATION: BELLEVUE, WA

SCALE: 1/8" = 1'-0"

DATE: 9/17/18

BY: [Signature]

PROJECT: BELLEVUE DOL TOWNHOMES

LOCATION: BELLEVUE, WA

SCALE: 1/8" = 1'-0"

DATE: 9/17/18

BY: [Signature]

PROJECT: BELLEVUE DOL TOWNHOMES

LOCATION: BELLEVUE, WA

SCALE: 1/8" = 1'-0"

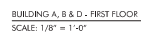
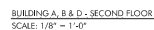
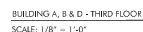
DATE: 9/17/18

BY: [Signature]

PROJECT: BELLEVUE DOL TOWNHOMES

LOCATION: BELLEVUE, WA

SCALE: 1/8" = 1'-0"





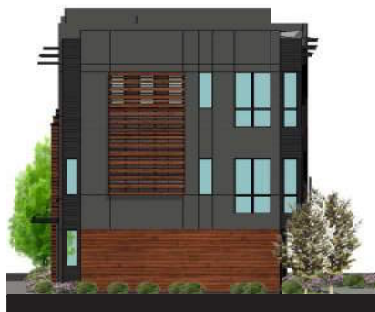
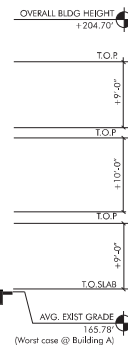
LEFT ELEVATION @ BUILDING D
SCALE: 1/8" = 1'-0"



LEFT ELEVATION @ BUILDINGS A & B
SCALE: 1/8" = 1'-0"



FRONT ELEVATION
SCALE: 1/8" = 1'-0"



RIGHT ELEVATION
SCALE: 1/8" = 1'-0"



REAR ELEVATION
SCALE: 1/8" = 1'-0"



PLAN 32

PLAN 3



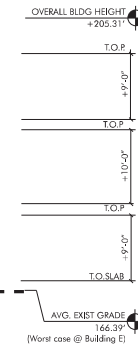
PLAN 3.2

PLAN 13

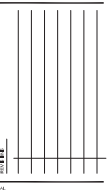


PLAN 32

PLAN 3



10900 NE 8th Street #1120
Bellevue, WA 98004
425.453.5388

PROJECT

BELLEVUE DOL TOWNHOMES
13133 NE BELLEVUE REDMOND RD
BELLEVUE, WA 98005

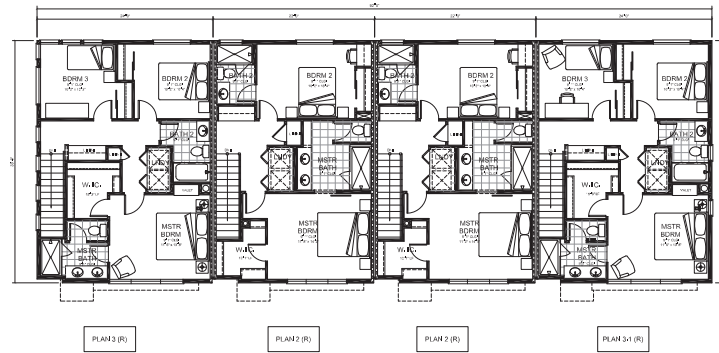
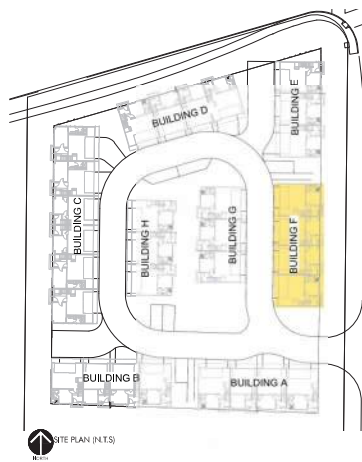
EMPLID	LIBERT
DOB	TRU_MGMT
REV	WEEK
DATE	09/17/2018
DAYL	548608
CADD	F

**BUILDING ELEVATIONS
BUILDINGS E, G, & H**

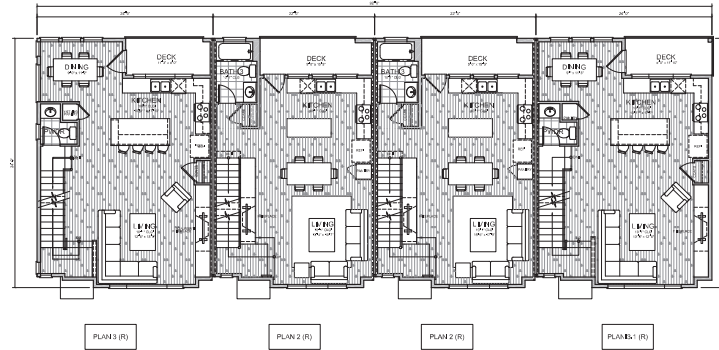
INDEX

A.5

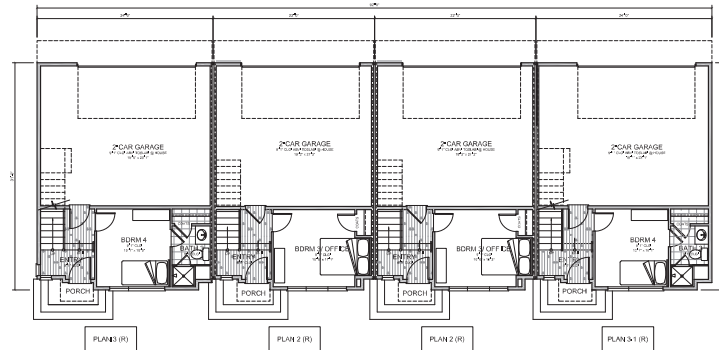
DESIGN REVIEW



BUILDING F - THIRD FLOOR
SCALE: 1/8" = 1'-0"



BUILDING F - SECOND FLOOR
SCALE: 1/8" = 1'-0"



BUILDING F - FIRST FLOOR
SCALE: 1/8" = 1'-0"



10900 NE 8th Street #1120
Bellevue, WA 98004
425-453-5388



PROJECT

BELLEVUE DOL TOWNHOMES
1315 NE BELLEVUE/REDMOND RD
BELLEVUE, WA 98005

DESIGNER	DAHLIN
ARCHITECT	DAHLIN
ENGINEER	DAHLIN
DATE	05/11/2018
PROJECT NO.	1400001
PROJECT NAME	BELLEVUE DOL TOWNHOMES

**BUILDING FLOOR PLANS
BUILDING F**

HEET

A.6

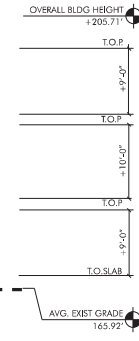
DESIGN REVIEW



LEFT ELEVATION
SCALE: 1/8" = 1'-0"



FRONT ELEVATION
SCALE: 1/8" = 1'-0"



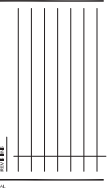
RIGHT ELEVATION
SCALE: 1/8" = 1'-0"



REAR ELEVATION
SCALE: 1/8" = 1'-0"



10000 NE 8th Street #1120
Bellevue, WA 98004
425-453-5388



1/8" = 1'-0"

PROJECT
BELLEVUE DOL TOWNHOMES
10000 NE 8th Street #1120
BELLEVUE, WA 98004

DESIGNED BY	DAHLIN
DRAWN BY	DAHLIN
CHECKED BY	DAHLIN
DATE	08/13/2018
PROJECT NO.	180001
CLIENT	DAHLIN
PROJECT NAME	BELLEVUE DOL TOWNHOMES

BUILDING ELEVATIONS
BUILDING F

DATE

A.7

DESIGN REVIEW



10900 NE 8th Street #1120
Bellevue, WA 98004
425-453-5388



PROJECT
BELLEVUE DOL TOWNHOMES
1313 NE BELLEVUE/REDMOND RD
BELLEVUE, WA 98005

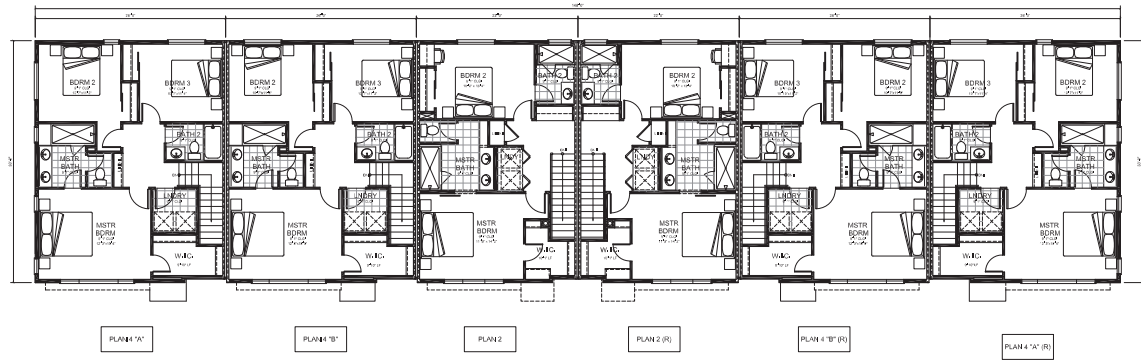
DESIGNER	DAHLIN
ARCHITECT	DAHLIN
ENGINEER	DAHLIN
DATE	01/11/2018
PROJECT	1000000001
DATE	01/11/2018

BUILDING FLOOR PLANS
BUILDING C

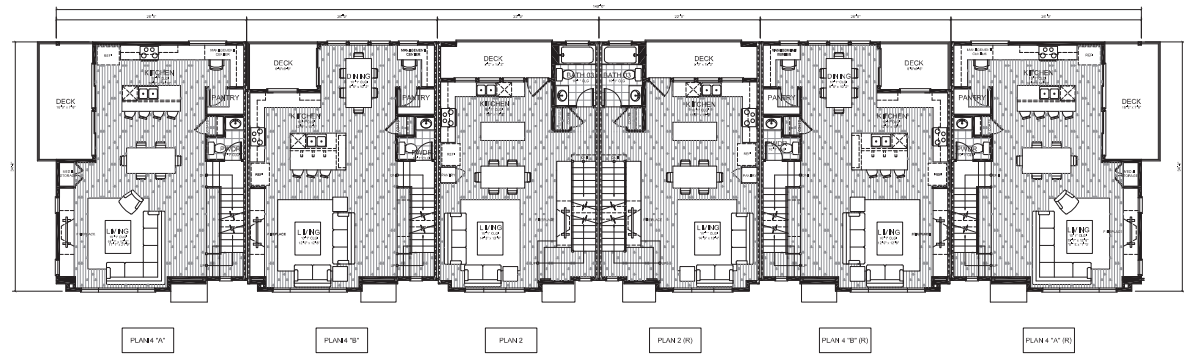
HEET

A.8

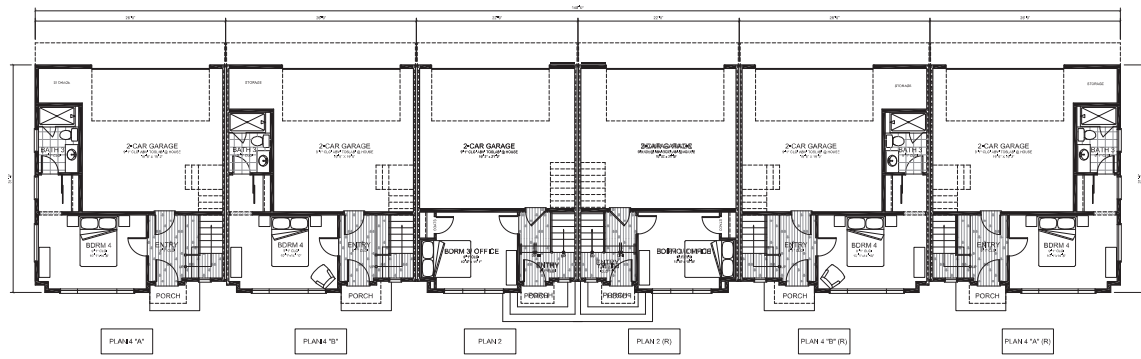
DESIGN REVIEW



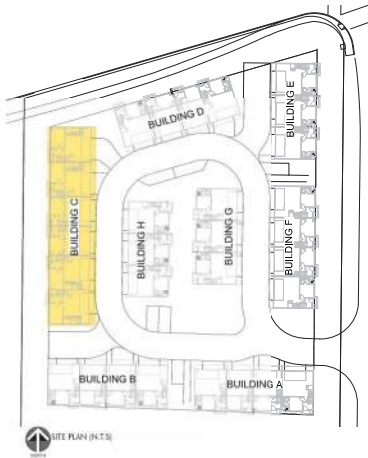
BUILDING C - THIRD FLOOR
SCALE: 1/8" = 1'-0"



BUILDING C - SECOND FLOOR
SCALE: 1/8" = 1'-0"



BUILDING C - FIRST FLOOR
SCALE: 1/8" = 1'-0"



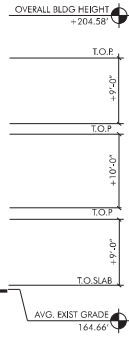
SITE PLAN (INTS)



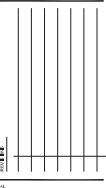
LEFT ELEVATION
SCALE: 1/8" = 1'-0"



FRONT ELEVATION
SCALE: 1/8" = 1'-0"



10000 NE 8th Street #1120
Bellevue, WA 98004
425-453-5388



SECTION



RIGHT ELEVATION
SCALE: 1/8" = 1'-0"



REAR ELEVATION
SCALE: 1/8" = 1'-0"

PROJECT

BELLEVUE DOL TOWNHOMES
10000 NE 8th Street #1120
BELLEVUE, WA 98004

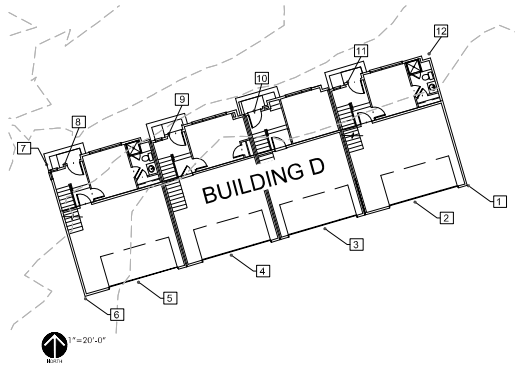
DESIGNED BY	DAHLIN
DRAWN BY	DAHLIN
CHECKED BY	DAHLIN
DATE	08/13/2018
PROJECT NO.	180001
PROJECT NAME	BELLEVUE DOL TOWNHOMES

BUILDING ELEVATIONS
BUILDING C

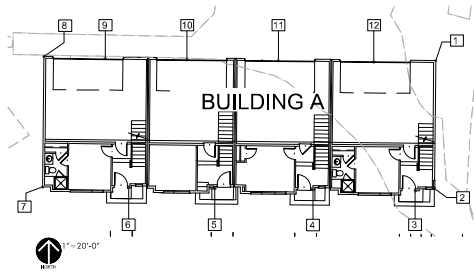
DATE

A.9

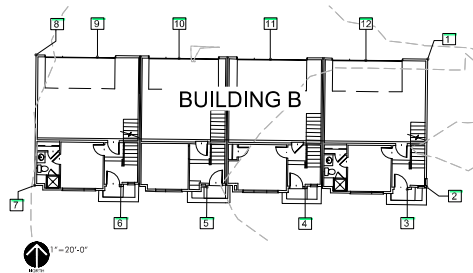
DESIGN REVIEW



Point #	Elevation
1	166.86
2	166.73
3	166.5
4	166.35
5	166.25
6	165.6
7	164.35
8	164.63
9	165.32
10	165.53
11	166.62
12	165.6
AVG	165.78



Point #	Elevation
1	164.26
2	163.39
3	163.19
4	162.49
5	162.39
6	162.32
7	162.29
8	162.26
9	162.88
10	163.06
11	163.24
12	163.79
AVG	163.015



Point #	Elevation
1	163.45
2	162.24
3	162.34
4	162.23
5	163.06
6	163.68
7	163.96
8	164.06
9	163.84
10	164.03
11	163.59
12	163.5
AVG	163.38





AVG 166.393

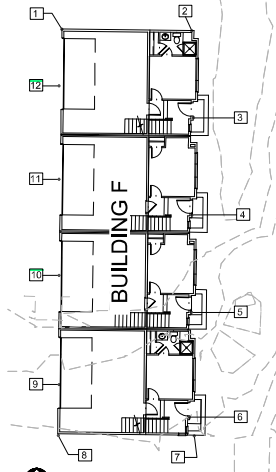


AVG	165.984
-----	---------



AVG	165.603
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Bldg F

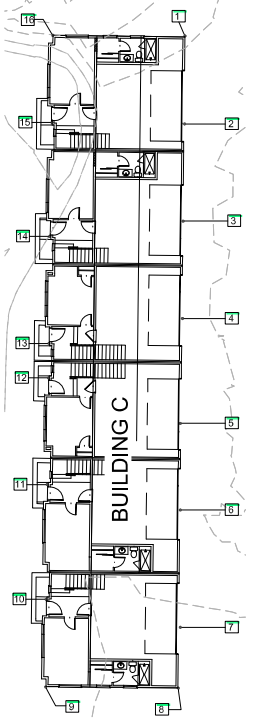
Point #	Elevation
1	166.83
2	166.62
3	166.75
4	166.58
5	165.63
6	165.06
7	163.53
8	163.93
9	164.53
10	166.63
11	166.08
12	166.73
AVG	165.7942



FRONT ELEVATION - BUILDING F
SCALE: 1/8" = 1'-0"

OVERALL BLDG HEIGHT
+205.71'

AVG. EXIST GRADE
165.92'



Bldg C

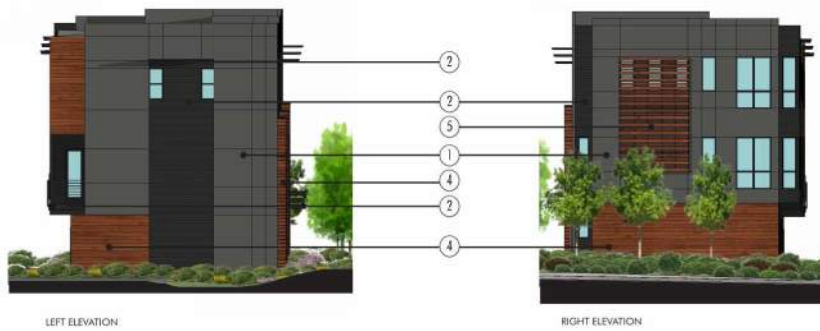
Point #	Elevation
1	163.87
2	164.53
3	164.83
4	164.77
5	164.62
6	164.44
7	163.91
8	163.72
9	164.22
10	164.67
11	164.29
12	165.44
13	164.56
14	165.05
15	166.6
16	164.07
AVG	164.6613



FRONT ELEVATION - BUILDING C
SCALE: 1/8" = 1'-0"

OVERALL BLDG HEIGHT
+204.58'

AVG. EXIST GRADE
164.56'







BCRA



2106 Pacific Avenue, Suite 300
Tacoma, WA 98402
T (253) 627-4367

November 7, 2018

City of Bellevue Development Services
Faheem Darab – Senior Planner
450 110th Ave NE
Bellevue, WA 98004

Dear Faheem,

The design of the proposed boardwalk system located in the right-of-way adjacent to the Bellevue DOL Townhomes project will meet the ADA design standards in the United States Access Board's Proposed Rights-of-Ways Guidelines.

The boardwalk design meets the following standards:

- Openings in ground and floor surfaces: Per the section 1017.6 'Openings' in the PROWAG, gaps between the treads on the boardwalk shall be limited to 1/2".
- Running slope & Cross slope: The running slope of the boardwalk will not exceed 5.0%, and the cross slope will not exceed 2.0%.
- Transitions between the boardwalk surface and the adjacent cement concrete sidewalk will be flush to prevent tripping hazards.
- Height of Boardwalk & Edge Protection: The height of the boardwalk (top of surface) will not exceed 30" above the adjacent finished grade. The boardwalk system will utilize a minimum 4" tall curb or barrier to provide edge protection.

Please refer to the attached boardwalk details for additional design information.

Sincerely,

2018.11.07
14:58:17-08'00'

Justin Goroch, PE
Senior Associate – Civil Engineering
BCRA

attachment

BCRADESIGN.COM





DEVELOPMENT SERVICES DEPARTMENT
ENVIRONMENTAL COORDINATOR
450 110th Ave NE
BELLEVUE, WA 98009-9012

DETERMINATION OF NON-SIGNIFICANCE

PROPONENT: Bellevue DOL Townhomes

LOCATION OF PROPOSAL: 13133 NE Bellevue-Redmond Road

DESCRIPTION OF PROPOSAL: Seeking Design Review approval for construction of 31 single-family attached (townhome) units.

FILE NUMBERS: 18-108498-LD

PLANNER: Faheem Darab

The Environmental Coordinator of the City of Bellevue has determined that this proposal does not have a probable significant adverse impact upon the environment. An Environmental Impact Statement (EIS) is not required under RCW 43.21C.030(2)(C). This decision was made after the Bellevue Environmental Coordinator reviewed the completed environmental checklist and information filed with the Land Use Division of the Development Services Department. This information is available to the public on request.

- ☐ There is no comment period for this DNS. There is a 14-day appeal period. Only persons who submitted written comments before the DNS was issued may appeal the decision. A written appeal must be filed in the City Clerk's office by 5:00 p.m. on _____.
- ☒ This DNS is issued after using the optional DNS process in WAC 197-11-355. There is no further comment period on the DNS. There is a 14-day appeal period. Only persons who submitted written comments before the DNS was issued may appeal the decision. A written appeal must be filed in the City Clerk's Office by 5 p.m. on **1/31/2019**
- ☐ This DNS is issued under WAC 197-11-340(2) and is subject to a 14-day comment period from the date below. Comments must be submitted by 5 p.m. on _____. This DNS is also subject to appeal. A written appeal must be filed in the City Clerk's Office by 5:00 p.m. on _____.

This DNS may be withdrawn at any time if the proposal is modified so as to have significant adverse environmental impacts; if there is significant new information indicating a proposals probable significant adverse environmental impacts (unless a non-exempt license has been issued if the proposal is a private project); or if the DNS was procured by misrepresentation or lack of material disclosure.

 **For Carol**
Environmental Coordinator **WOLAND**

1/17/2019
Date

OTHERS TO RECEIVE THIS DOCUMENT:

- ☒ State Department of Fish and Wildlife / Stewart.Reinbold@dfw.gov; Christa.Heller@dfw.wa.gov;
- ☒ Army Corps of Engineers Susan.M.Powell@nws02.usace.army.mil
- ☒ Attorney General ecvolyef@atg.wa.gov
- ☒ Muckleshoot Indian Tribe Karen.Walter@muckleshoot.nsn.us; Fisheries.fileroom@muckleshoot.nsn.us



DEVELOPMENT SERVICES DEPARTMENT
450 110TH AVENUE NE
BELLEVUE, WA 98009-9012

Initial review by Faheem Darab, 04/06/18

SEPA Environmental Checklist

If you need assistance in completing the checklist or have any questions regarding the environmental review process, please visit the Land Use Desk in the Permit Center between 8 a.m. and 4 p.m., Monday through Friday (Wednesday, 10 to 4) or call or email the Land Use Division at 425-452-4188 or landusereview@bellevuewa.gov. Assistance for the hearing impaired: Dial 711 (Telecommunications Relay Service).

Purpose of checklist:

The City of Bellevue uses this checklist to help determine whether the environmental impacts of your proposal are significant. This information is also helpful to determine if available avoidance, minimization or compensatory mitigation measures will address the probable significant impacts or if an environmental impact statement will be prepared to further analyze the proposal.

Instructions for applicants:

This environmental checklist asks you to describe some basic information about your proposal. Please answer each question accurately and carefully, to the best of your knowledge. You may need to consult with an agency specialist or private consultant for some questions. You may use "not applicable" or "does not apply" only when you can explain why it does not apply and not when the answer is unknown. You may also attach or incorporate by reference additional studies and reports. Please make complete and accurate answers to these questions to the best of your ability in order to avoid delays.

The checklist questions apply to all parts of your proposal, even if you plan to do them over a period of time or on different parcels of land. Attach any additional information that will help describe your proposal or its environmental effects. The City may ask you to explain your answers or provide additional information reasonably related to determining if there may be significant adverse impact.

PLEASE REMEMBER TO SIGN THE CHECKLIST. Electronic signatures are also acceptable.

Received
MAR 19 2018
Permit Processing

A. Background [\[help\]](#)

- ✓ 1. Name of proposed project, if applicable: [\[help\]](#)
Bellevue DOL Townhomes
- ✓ 2. Name of applicant: [\[help\]](#)
Bel-Red USA Limited Partnership / MJS Investors
- ✓ 3. Address and phone number of applicant and contact person: [\[help\]](#)
MJS Investors; Jeff Miller, 425-891-1074
- ✓ 4. Date checklist prepared: [\[help\]](#)
March 6, 2018
- ✓ 5. Agency requesting checklist: [\[help\]](#)
City of Bellevue, Development Services Department
- ✓ 6. Proposed timing or schedule (including phasing, if applicable): [\[help\]](#)
Construction will begin when permits are in hand.
- ✓ 7. Do you have any plans for future additions, expansion, or further activity related to or connected with this proposal? If yes, explain. [\[help\]](#)
No future plans.
- ✓ 8. List any environmental information you know about that has been prepared, or will be prepared, directly related to this proposal. [\[help\]](#)
ALTA Survey; Lanktree; 7/6/2017
Geotechnical Report; Terra Associates, Inc.; March 2, 2018
Phase I Environmental Site Assessment; Terra Associates; July 11, 2017
Trip Generation Memo; Transpo Group; October 27, 2017
- ✓ 9. Do you know whether applications are pending for governmental approvals of other proposals directly affecting the property covered by your proposal? If yes, explain. [\[help\]](#)
None known.
- ✓ 10. List any government approvals or permits that will be needed for your proposal, if known. [\[help\]](#)
Typical building and site development permits
Design Review
Developer extension agreements for water, sewer, and storm
- ✓ 11. Give brief, complete description of your proposal, including the proposed uses and the size of the project and site. There are several questions later in this checklist that ask you to describe certain aspects of your proposal. You do not need to repeat those answers on this page. (Lead agencies may modify this form to include additional specific information on project description.) [\[help\]](#)
Demolish existing single story structure and existing site improvements. Construct 8 3-story multifamily residential buildings, for a total of 31 homes, with interior garage parking spaces in each unit, and related site paving, landscaping,

utilities, and other improvements.

- ✓ 12. Location of the proposal. Give sufficient information for a person to understand the precise location of your proposed project, including a street address, if any, and section, township, and range, if known. If a proposal would occur over a range of area, provide the range or boundaries of the site(s). Provide a legal description, site plan, vicinity map, and topographic map, if reasonably available. While you should submit any plans required by the agency, you are not required to duplicate maps or detailed plans submitted with any permit applications related to this checklist. [\[help\]](#)

13133 NE Bellevue-Redmond Road; Parcel 2825059084

LOT 1 CITY OF BELLEVUE SHORT PLAT NO 77-14 REC NO 7702151114 SD SP
DAF: BEG AT SE COR OF STR 28-25-5 TH N ALG E LN THOF 1109.14 FT TH N
87-55-48 W 30.01 FT TO W MGN 132ND AVE SE TO TPOB TH CONTG N 87-55-48
W 240.07 FT TH N 00-36-54 E 557.21 FT TO SLY MGN BELLEVUE-REDMOND RD
TH NELY ALG SD SLY MGN TO W LN OF 132ND AVE NE TH S ALG SD W LN TO
TPOB

B. Environmental Elements [\[help\]](#)

1. Earth [\[help\]](#)

- ✓ a. General description of the site: [\[help\]](#) (select one): ☒ Flat, ☐ rolling, ☐ hilly, ☐ steep slopes, ☐ mountainous, other: *The site sits several feet above the adjacent streets and is somewhat level in the center of the property with slopes down along all the property edges.*
- ✓ b. What is the steepest slope on the site (approximate percent slope)? [\[help\]](#)
Steepest slope on the site is in the southeast corner and is approximately 60%. It appears to be the result of previous grading for the existing structure and parking lot.
- ✓ c. What general types of soils are found on the site (for example, clay, sand, gravel, peat, muck)? If you know the classification of agricultural soils, specify them and note any agricultural land of long-term commercial significance and whether the proposal results in removing any of these soils. [\[help\]](#)
Per the Geotech Report, the soils are mapped as Recessional Outwash (Qvr) and Glacial Till (Qvt). Observed soil conditions are generally consistent with the mapped geology.
- ✓ d. Are there surface indications or history of unstable soils in the immediate vicinity? If so, describe. [\[help\]](#)
There are no indications of unstable soils in the vicinity.
- ✓ e. Describe the purpose, type, total area, and approximate quantities and total affected area of any filling, excavation, and grading proposed. Indicate source of fill. [\[help\]](#)
*Cut: 6000 cy
Fill: 400 cy. Fill will consist of both native soils and structural fill. Off-site fill will be provided from an approved source.*

- f. Could erosion occur as a result of clearing, construction, or use? If so, generally describe. [\[help\]](#)

It is possible that some erosion could occur as site development activities are on-going. Construction activities including stripping and grading will expose soils to the erosion effects of wind and water. Wet weather construction could increase the amount and extent of erosion and potential sedimentation.

- g. About what percent of the site will be covered with impervious surfaces after project construction (for example, asphalt or buildings)? [\[help\]](#)

Proposed total site coverage is approximately 60%.

- h. Proposed measures to reduce or control erosion, or other impacts to the earth, if any: [\[help\]](#)

A temporary erosion control plan will be prepared and implemented during the construction phase in accordance with City of Bellevue Development Standards. TESC measures will include a temporary construction entrance, filter fabric fence, temporary drainage ditches, straw bales and catch basin protection.

2. Air [\[help\]](#)

- a. What types of emissions to the air would result from the proposal during construction, operation, and maintenance when the project is completed? If any, generally describe and give approximate quantities if known. [\[help\]](#)

Emissions during construction are anticipated as a direct result of the construction workers use of personal, company and/or subcontractor vehicles on-site and travel to and from the site. Once the project is complete, automobile exhaust from residents' vehicles will be the main source of emissions.

- b. Are there any off-site sources of emissions or odor that may affect your proposal? If so, generally describe. [\[help\]](#)

No off-site emissions are anticipated to affect this proposal.

- c. Proposed measures to reduce or control emissions or other impacts to air, if any: [\[help\]](#)

Dust will be controlled through watering during the drier months of construction as needed. No other measures are planned.

3. Water [\[help\]](#)

- a. Surface Water:

- 1) Is there any surface water body on or in the immediate vicinity of the site (including year-round and seasonal streams, saltwater, lakes, ponds, wetlands)? If yes, describe type and provide names. If appropriate, state what stream or river it flows into. [\[help\]](#)

Goff Creek is located to the east and south of this area, undergrounded in some areas and as it crosses roads. This creek is classified as a fish bearing stream, Type F. There is an associated wetland on the east side of 132nd Street and in the parcel to the south. The creek has a 100 foot buffer and a 20 foot structure setback. The wetland has 75 foot buffer and 20 foot structure setback. The development and structures for this proposal are outside the buffers and setbacks.

- 2) Will the project require any work over, in, or adjacent to (within 200 feet) the described waters? If yes, please describe and attach available plans. [\[help\]](#)
Yes work will occur within 200 feet of the wetlands however it will be outside of the required buffers and setbacks.
- 3) Estimate the amount of fill and dredge material that would be placed in or removed from surface water or wetlands and indicate the area of the site that would be affected. Indicate the source of fill material. [\[help\]](#)
No fill or dredging will occur in waters.
- 4) Will the proposal require surface water withdrawals or diversions? Give general description, purpose, and approximate quantities if known. [\[help\]](#)
No surface water withdrawals or diversions are proposed.
- 5) Does the proposal lie within a 100-year floodplain? If so, note location on the site plan. [\[help\]](#)
This is not within a 100-year floodplain according to FEMA mapping.
- 6) Does the proposal involve any discharges of waste materials to surface waters? If so, describe the type of waste and anticipated volume of discharge. [\[help\]](#)
No waste materials are expected to be discharged to surface waters.

b. Ground Water:

- 1) Will groundwater be withdrawn from a well for drinking water or other purposes? If so, give a general description of the well, proposed uses and approximate quantities withdrawn from the well. Will water be discharged to groundwater? Give general description, purpose, and approximate quantities if known. [\[help\]](#)
No wells are proposed, nor will water be discharged to groundwater. On-site explorations encountered water at about seven feet below existing grade.
- 2) Describe waste material that will be discharged into the ground from septic tanks or other sources, if any (for example: Domestic sewage; industrial, containing the following chemicals...; agricultural; etc.). Describe the general size of the system, the number of such systems, the number of houses to be served (if applicable), or the number of animals or humans the system(s) are expected to serve. [\[help\]](#)
No waste materials will be discharged to ground. Property

is served by public sanitary and storm sewers.

c. Water runoff (including stormwater):

- 1) Describe the source of runoff (including storm water) and method of collection and disposal, if any (include quantities, if known). Where will this water flow? Will this water flow into other waters? If so, describe. [\[help\]](#)

The source of runoff will be storm water runoff from building roof tops, concrete walks, asphalt paved areas, and landscaped areas. Runoff will be collected by roof downspouts and catch basins and conveyed by underground storm pipe to a proprietary water quality treatment system. Runoff will then be conveyed via underground pipe to an underground detention tank system prior to discharging to the existing public storm water system located in 132nd.

- 2) Could waste materials enter ground or surface waters? If so, generally describe. [\[help\]](#)

Runoff from the site will be treated with a proprietary water quality treatment system that will remove contaminants associated runoff from pollution generating impervious surfaces and pervious surfaces such as parking lots, drive aisles, driveways, and chemically treated landscaping.

- 3) Does the proposal alter or otherwise affect drainage patterns in the vicinity of the site? If so, describe. [\[help\]](#)

This proposal does not intend to alter area drainage patterns.

d. Proposed measures to reduce or control surface, ground, and runoff water, and drainage pattern impacts, if any: [\[help\]](#)

During the construction phase, temporary erosion control measures, ongoing maintenance, soil stabilization and other best management practices will be implemented to help reduce and control impacts from the project. Permanent measures to reduce and control runoff from the completed project will include catch basins, underground conveyance pipe, detention and water quality treatment as determined necessary.

4. Plants [\[help\]](#)

- a. Check the types of vegetation found on the site: [\[help\]](#)

☒deciduous tree: alder, maple, aspen, other: [Click here to enter text.](#)

☒evergreen tree: fir, cedar, pine, other: [Click here to enter text.](#)

☒shrubs

☐grass

☐pasture

☐crop or grain

☐ Orchards, vineyards or other permanent crops.

☐ wet soil plants: cattail, buttercup, bullrush, skunk cabbage, other: *Click here to enter text.*

☐ water plants: water lily, eelgrass, milfoil, other: *Click here to enter text.*

☐ other types of vegetation: *Click here to enter text.*

- b. ☒ What kind and amount of vegetation will be removed or altered? [\[help\]](#)

The site will be cleared of vegetation with the exception of those trees and shrubs that may be saved within the property line-to-building setback areas.

- c. ☒ List threatened and endangered species known to be on or near the site. [\[help\]](#)

None known.

- d. ☒ Proposed landscaping, use of native plants, or other measures to preserve or enhance vegetation on the site, if any: [\[help\]](#)

Landscaping along property perimeters and internal to the site will be designed and installed in accordance with applicable City landscaping and buffer requirements. Significant trees internal to the site perimeter landscape requirements will either be saved where possible, or replaced per city code replacement standards. Native plants will be proposed.

- e. ☒ List all noxious weeds and invasive species known to be on or near the site. [\[help\]](#)

English Ivy and possibly blackberry

5. Animals [\[help\]](#)

- a. ☒ List any birds and other animals which have been observed on or near the site or are known to be on or near the site. [\[help\]](#)

Examples include:

birds: ☐ hawk, ☐ heron, ☐ eagle, ☐ songbirds, other: *birds typical of suburban environments such as jays, crows, sparrows etc. are likely to be seen on or near site.*

mammals: ☐ deer, ☐ bear, ☐ elk, ☐ beaver, other: *small mammals typical of suburban environments such as rodents/squirrels, raccoons are likely to be seen on or near the site.*

fish: ☐ bass, ☐ salmon, ☐ trout, ☐ herring, ☐ shellfish, other: *none on or near the site, but do occur within local creeks.*

- b. ☒ List any threatened and endangered species known to be on or near the site. [\[help\]](#)

None known.

- c. ☒ Is the site part of a migration route? If so, explain. [\[help\]](#)

The City of Bellevue is within the Pacific Flyway for migratory birds. Migrating species of geese and ducks can be found in lakes, ponds, wetlands and waterways in the area.

- d. Proposed measures to preserve or enhance wildlife, if any: [\[help\]](#)
Trees around the perimeter of the site will remain.
- e. List any invasive animal species known to be on or near the site. [\[help\]](#)
Not known.

6. Energy and Natural Resources [\[help\]](#)

- a. What kinds of energy (electric, natural gas, oil, wood stove, solar) will be used to meet the completed project's energy needs? Describe whether it will be used for heating, manufacturing, etc. [\[help\]](#)
Electric - Power and lighting
Natural Gas - HVAC
- b. Would your project affect the potential use of solar energy by adjacent properties? If so, generally describe. [\[help\]](#)
The proposed project will not adversely affect the potential use of solar energy by the adjacent properties as the property is significantly separated to the north by Bel-Red Road and the east by 132nd Ave NE. Also, there will still be trees remaining between the proposed buildings and the property lines and so the buildings will not change the existing ability of adjacent sites to use solar energy. Solar use is generally not impacted to the south or west.
- c. What kinds of energy conservation features are included in the plans of this proposal? List other proposed measures to reduce or control energy impacts, if any: [\[help\]](#)
Washington State energy requirements will be met when designing building shell, lighting, heating, and ventilation equipment.

7. Environmental Health [\[help\]](#)

- a. Are there any environmental health hazards, including exposure to toxic chemicals, risk of fire and explosion, spill, or hazardous waste, that could occur as a result of this proposal? If so, describe. [\[help\]](#)
A Phase I was done for the property and it was determined that there are no REC's associated with the site or nearby and no high risk businesses are in the immediate vicinity.
 - 1) Describe any known or possible contamination at the site from present or past uses. [\[help\]](#)
None known.
 - 2) Describe existing hazardous chemicals/conditions that might affect project development and design. This includes underground hazardous liquid and gas transmission pipelines located within the project area and in the vicinity. [\[help\]](#)
Nearby sites were studied through a Phase I. The Bellevue Plating Company, Circuit Services World Wide, JB Carpets

and Wilson Transmissions, were all studied. Underground storage tanks were also reviewed along with other possible sources of contamination. There were no determined REC's for this site.

- 3) Describe any toxic or hazardous chemicals that might be stored, used, or produced during the project's development or construction, or at any time during the operating life of the project. [\[help\]](#)

Various chemicals will likely be present in the proposed buildings, although they would be expected to be present in quantities typical to the residential activities. Overall, any chemicals should be used as necessary, and any un-used or waste materials properly recycled/disposed of.

- 4) Describe special emergency services that might be required. [\[help\]](#)
None anticipated.

- 5) Proposed measures to reduce or control environmental health hazards, if any: [\[help\]](#)
None proposed.

b. Noise [\[help\]](#)

- 1) What types of noise exist in the area which may affect your project (for example: traffic, equipment, operation, other)? [\[help\]](#)

Local street traffic noise and that from nearby businesses.

- 2) What types and levels of noise would be created by or associated with the project on a short-term or a long-term basis (for example: traffic, construction, operation, other)?
Indicate what hours noise would come from the site. [\[help\]](#)

Short-term noise would result from construction activities. Long term noises associated with the proposed project will include mostly the sound of residents entering and leaving the property and occasional sounds from use of community open space.

- 3) Proposed measures to reduce or control noise impacts, if any: [\[help\]](#)
None proposed.

Project shall comply with B.C.C.
on noise control 9.18.

City requires the best available and
feasible noise abatement technology to mitigate construction
noise impacts to surrounding uses.

8. Land and Shoreline Use [\[help\]](#)

- a. What is the current use of the site and adjacent properties? Will the proposal affect current land uses on nearby or adjacent properties? If so, describe. [\[help\]](#)

The current use of the site is as an Office building for Department of Licensing. To the north and all along Bel-Red road is commercial and offices. Directly south is mostly single family residential. Multi-family housing is mixed into the area on the south side of Bel-Red.

- b. Has the project site been used as working farmlands or working forest lands? If so, describe. How much agricultural or forest land of long-term commercial significance will be

converted to other uses as a result of the proposal, if any? If resource lands have not been designated, how many acres in farmland or forest land tax status will be converted to nonfarm or nonforest use? [\[help\]](#)

No working farmlands or forest lands.

- 1) Will the proposal affect or be affected by surrounding working farm or forest land normal business operations, such as oversize equipment access, the application of pesticides, tilling, and harvesting? If so, how: [\[help\]](#)

There are no nearby working farmlands or forest lands.

- c. Describe any structures on the site. [\[help\]](#)

Single story, 12,050 SF masonry office building constructed in 1978.

- d. Will any structures be demolished? If so, what? [\[help\]](#)

Yes. This office building will be removed.

- e. What is the current zoning classification of the site? [\[help\]](#)

*Bel-Red-Office, Residential, Transition Design District;
BR-ORT*

- f. What is the current comprehensive plan designation of the site? [\[help\]](#)

Bel-Red Office, Residential Transition; BR-ORT

- g. If applicable, what is the current shoreline master program designation of the site? [\[help\]](#)

Not applicable

- h. Has any part of the site been classified as a critical area by the city or county? If so, specify. [\[help\]](#)

None of the site is classified as a critical area.

- i. Approximately how many people would reside or work in the completed project? [\[help\]](#)

31 townhomes are proposed.

- j. Approximately how many people would the completed project displace? [\[help\]](#)

Approximately 20-30 full- and part-time staff.

- k. Proposed measures to avoid or reduce displacement impacts, if any: [\[help\]](#)

The Driver's Licensing office staff may be able to transfer to other offices.

- l. Proposed measures to ensure the proposal is compatible with existing and projected land uses and plans, if any: [\[help\]](#)

The proposed development is allowed by city code and will be designed to be in compliance with the city development standards and will be required to go through the city's Design Review process. All significant trees within building setbacks that do not constitute a safety hazard shall be retained.

Project shall comply with all relevant Land Use, Clear and Grade, Building, Fire, Transportation and Utilities Codes.

- m. Proposed measures to ensure the proposal is compatible with nearby agricultural and forest lands of long-term commercial significance, if any: [\[help\]](#)
There are no nearby agricultural or forest lands.

9. Housing [\[help\]](#)

- a. Approximately how many units would be provided, if any? Indicate whether high, middle, or low-income housing. [\[help\]](#)
31 townhomes of mid-income housing are proposed.
- b. Approximately how many units, if any, would be eliminated? Indicate whether high, middle, or low-income housing. [\[help\]](#)
No residences will be eliminated with this proposal.
- c. Proposed measures to reduce or control housing impacts, if any: [\[help\]](#)
None proposed.

10. Aesthetics [\[help\]](#)

- a. What is the tallest height of any proposed structure(s), not including antennas; what is the principal exterior building material(s) proposed? [\[help\]](#)
No structure will be taller than the maximum height allowed of 45 feet.
- b. What views in the immediate vicinity would be altered or obstructed? [\[help\]](#)
No views will be altered or obstructed by this proposal as there are significant trees around the edges of this property.
- c. Proposed measures to reduce or control aesthetic impacts, if any: [\[help\]](#)
Building and site design will be reviewed through city's Design Review process.

11. Light and Glare [\[help\]](#)

- a. What type of light or glare will the proposal produce? What time of day would it mainly occur? [\[help\]](#)
On site lighting will be directed downward and away from adjacent uses while creating enough illumination to address residents' security and safety. Light from residences, and from vehicle headlights as cars navigate the parking area, will be emitted from the site during evening and morning hours.
- b. Could light or glare from the finished project be a safety hazard or interfere with views? [\[help\]](#)
It is not expected that glare from the proposed project will create a safety hazard or interfere with views.

- c. What existing off-site sources of light or glare may affect your proposal? [\[help\]](#)

The two street luminaires on the north property line, and traffic lights on Bel-Red Road, will have a some impact on the northern residences. Trees along the property edge will help screen some light.

- d. Proposed measures to reduce or control light and glare impacts, if any: [\[help\]](#)

Additional landscaping along the north property line may be considered. Perimeter landscaping will be provided. Exterior lighting will utilize cutoff shields or equivalent technique to mitigate glare and light impacts.

12. Recreation [\[help\]](#)

- a. What designated and informal recreational opportunities are in the immediate vicinity? [\[help\]](#)

There are various athletic facilities in the near vicinity: Northwest CrossFit, Sleeper Athletics, Balanced Bodyworks, WA Shotokan Karate, Bellevue Tennis Academy, Rain city Fencing, & Seattle Pacific Table Tennis Club, to name a few. There are also several parks for more passive recreation: Highland Park, Kelsey Creek Park and Bellevue Botanical Garden. Plus golf at the Glendale Country Club. All these are anywhere from under 0.25 mile to 1.5 miles from the site.

- b. Would the proposed project displace any existing recreational uses? If so, describe. [\[help\]](#)

No recreational use would be displaced.

- c. Proposed measures to reduce or control impacts on recreation, including recreation opportunities to be provided by the project or applicant, if any: [\[help\]](#)

An outdoor children's play area of at least 800 SF will be provided in the interior of the project site. Landscape requirements also require additional outdoor area of a minimum of 1850 SF based on unit count. Approximately 2200 SF is anticipated to be provided with the proposed site plan.

13. Historic and cultural preservation [\[help\]](#)

- a. Are there any buildings, structures, or sites, located on or near the site that are over 45 years old listed in or eligible for listing in national, state, or local preservation registers located on or near the site? If so, specifically describe. [\[help\]](#)

There are nearby buildings that are over 45 years old but none that have been determined to be eligible for historical recording.

- b. Are there any landmarks, features, or other evidence of Indian or historic use or occupation? This may include human burials or old cemeteries. Are there any material evidence, artifacts, or areas of cultural importance on or near the site? Please list any professional studies conducted at the site to identify such resources. [\[help\]](#)

There are no known landmarks or evidence of Indian or historic use in the nearby area.

- c. Describe the methods used to assess the potential impacts to cultural and historic resources on or near the project site. Examples include consultation with tribes and the department of archeology and historic preservation, archaeological surveys, historic maps, GIS data, etc. [\[help\]](#)

No records were found utilizing the Washington Information System or Architectural & Archaeological Records Data (WISAARD) system website for this property.

- d. Proposed measures to avoid, minimize, or compensate for loss, changes to, and disturbance to resources. Please include plans for the above and any permits that may be required. [\[help\]](#)

In the event that suspected historic or cultural artifacts, or objects of suspected archaeological value are discovered during site excavation, grading or other form of site development/construction, all work on site shall immediately stop and the property owner/developer shall notify the City, the State Department of Archaeology and Historic Preservation (DAHP) and local Indian Tribes.

14. Transportation [\[help\]](#)

- a. Identify public streets and highways serving the site or affected geographic area and describe proposed access to the existing street system. Show on site plans, if any. [\[help\]](#)
Currently both Bel-Red Road and 132nd Ave NE have access drives into the site. The proposed site would eliminate the Bel-Red access. Nearby highways are I-405 and hwy 520.

- b. Is the site or affected geographic area currently served by public transit? If so, generally describe. If not, what is the approximate distance to the nearest transit stop? [\[help\]](#)
Bus service exists for Routes 226 & 888 along Bel-Red Road, with stops on both sides at the intersection of Bel-Red and 132nd.

- c. How many additional parking spaces would the completed project or non-project proposal have? How many would the project or proposal eliminate? [\[help\]](#)
The project will eliminate all 86 existing on-site parking. Project proposal includes internal 2-car garages for each unit and 4 guest parking spaces.

- d. Will the proposal require any new or improvements to existing roads, streets, pedestrian, bicycle or state transportation facilities, not including driveways? If so, generally describe (indicate whether public or private). [\[help\]](#)
City of Bellevue will require half-street frontage improvements on 132nd Ave and street trees, curb, gutter and sidewalk on Bel-Red, along with frontage easement dedications for this project.

- e. Will the project or proposal use (or occur in the immediate vicinity of) water, rail, or air transportation? If so, generally describe. [\[help\]](#)
This project will not use water, rail, or air transportation.

- f. How many vehicular trips per day would be generated by the completed project or proposal? If known, indicate when peak volumes would occur and what percentage of the volume would be trucks (such as commercial and nonpassenger vehicles). What data or transportation models were used to make these estimates? [\[help\]](#)

Comparing the trip generation estimates for the existing and proposed uses, the proposed project would generate at least 187 fewer PM peak hour vehicle trips. This proposal would have 19 weekday PM peak hour vehicle trips. Please refer to the Trip Generation Memo.

- g. Will the proposal interfere with, affect or be affected by the movement of agricultural and forest products on roads or streets in the area? If so, generally describe. [\[help\]](#)

This project will not affect the movement of agricultural or forest products.

- h. Proposed measures to reduce or control transportation impacts, if any: [\[help\]](#)

None proposed. Frontage improvements along Bel-red Road and 132nd Avenue NE will improve pedestrian and bicycle infrastructure, potentially improving vehicular flow through separation of modes of travel.

15. Public Services [\[help\]](#)

- a. Would the project result in an increased need for public services (for example: fire protection, police protection, public transit, health care, schools, other)? If so, generally describe. [\[help\]](#)

Fire, police and ambulance services would be required as would be expected for a multi-family housing development of this type and size.

- b. Proposed measures to reduce or control direct impacts on public services, if any. [\[help\]](#)

None proposed.

16. Utilities [\[help\]](#)

- a. Circle utilities currently available at the site: [\[help\]](#)
electricity, natural gas, water, refuse service, telephone, sanitary sewer, septic system, other

Electricity, natural gas, water, refuse service, telephone, sanitary sewer, are all currently available at the site.

- c. Describe the utilities that are proposed for the project, the utility providing the service, and the general construction activities on the site or in the immediate vicinity which might be needed. [\[help\]](#)

The proposed building will require all common utilities currently provided at the subject site.

Electricity - Puget Sound Energy

Gas - Puget Sound Energy

Water service - City of Bellevue

Sanitary Sewer - City of Bellevue

Storm Sewer - City of Bellevue

Garbage & Recycling - Republic Services

C. Signature [\[help\]](#)

The above answers are true and complete to the best of my knowledge. I understand that the lead agency is relying on them to make its decision.

Signature: Christine Phillips

Name of signee: *Christine Phillips*

Position and Agency/Organization: *Senior Planner / BCRA*

Date Submitted: *March 6, 2018*

AFD 04/06/18